

**Appendix B: Parking Agreement, Transportation Demand Management Plan,
Site Plan, Email Communication regarding Valet Parking**

PARKING AGREEMENT
BETWEEN THE CITY OF SAN JOSE
AND
ALMADEN CORNER, LLC
FOR THE USE OF THE MARKET/SAN PEDRO SQUARE GARAGE

This Parking Agreement ("Agreement") is made this _____ **day of** _____ ("Effective Date"), by and between the CITY OF SAN JOSE, a municipal corporation of the State of California ("City"), and Almaden Corner, LLC, a California company authorized to do business in California ("Developer"). Each of City and Developer are sometimes hereinafter referred to as a "Party" and collectively as the "Parties."

RECITALS

WHEREAS, the Developer has plans to build a hotel consisting of 272 hotel rooms ("Hotel"); and

WHEREAS, the Hotel will be located on a 0.2 acre site on the southwest corner of the block bounded by West Santa Clara Street, North Almaden Boulevard, Carlisle Street, and Notre Dame Avenue; and

WHEREAS, construction activity associated with the Hotel is tentatively scheduled to occur approximately between May 2020 and May 2022; and

WHEREAS, the Hotel development will not include onsite parking; and

WHEREAS, Developer wishes to secure access to parking permits to satisfy various parking requirements which will enable the Hotel to provide Hotel guests access to proximate parking; and

WHEREAS, City and Developer have had discussions regarding the Hotel development in Downtown San José, and the Developer has requested assistance from the City with providing parking on a long term basis for Hotel guests; and

WHEREAS, City and Developer now desire to enter into this Agreement to set forth the terms and conditions pursuant to which City will issue up to forty-one (41) parking permits to Developer for use by Hotel to park in the Market/San Pedro Square Garage for up to thirty (30) years; and

WHEREAS, City's Director of the Department of Transportation ("Director"), or designee, is charged with the administration of this Agreement.

NOW THEREFORE, in consideration of the mutual conditions and covenants set forth in this Agreement and for valuable consideration, receipt and sufficiency of which is hereby acknowledged, the Parties hereby agree as follows:

SECTION 1. **DEFINITIONS**

For the purposes of this Agreement, the definitions in this Section 1 shall govern the application and interpretation of this Agreement.

- A. *Hotel* means the planned hotel to be located at 8 N. Almaden Boulevard and currently planned to contain 272 guest rooms.
- B. *Market/San Pedro Square Garage* or *Parking Facility* means the municipal off-street parking facility currently owned by City and situated in San José in the block bounded by Santa Clara Street, Market Street, Saint John Street, and San Pedro Street, located at 45 North Market Street.
- C. *Monthly Rate* means the parking rate required to be paid by the Developer to the City under this Agreement for each Parking Permit as further described in Section 4.
- D. *Operator* means the person or entity under a written agreement with the City to manage and operate the Parking Facility on behalf of the City. The current Operator of the Parking Facility is SP Plus (SP+) and may be changed from time to time at the sole discretion of City.
- E. *Parking Permit* means the credential or electronic access issued by the City to the Developer to access and park in the Parking Facility pursuant to this Agreement.
- F. *Permit Holders* means all persons or entities which hold one or more valid permits to park in the Parking Facility.
- F. *Phase I Development Holding Period* means the period that commences on the Effective Date, as described in Section 4, where the Developer shall pay City the Prevailing Market Rate for each Parking Permit to be issued during the Phase II Hotel Occupancy Period. Concurrent with the commencement of the Phase I Development Holding Period, the Parties shall execute a one-page form as set forth in Exhibit A setting forth and confirming the commencement and expiration dates of the Phase I Development Holding Period.
- G. *Phase II Hotel Occupancy Period* means the period commencing immediately following the expiration of the Phase I Development Holding Period and continuing through the remaining Term of this Agreement where Developer is required to pay City for use of the Parking Permits as outlined in Section 4.
- H. *Prevailing Market Parking Rate* means the monthly permit rate for access exclusively to the Market/San Pedro Square Garage outlined in the Master Parking Rate Schedule which may be amended from time to time.

SECTION 2. **TERM**

This Agreement shall commence on the Effective Date and be broken into two phases as outlined below, but in no case shall the total term of the agreement extend beyond 35 years from the Effective Date.

Phase I Development Holding Period shall be initiated upon the commencement of the Agreement on the Effective Date until such time as the Hotel receives its Certificate of Occupancy from the City. This period of time shall provide Developer sufficient time to finalize development plans and construction of the Hotel. The maximum term for the Phase I Development Holding Period shall be five (5) years.

Phase II Hotel Occupancy Period shall commence upon the City’s issuance of the Certificate of Occupancy and shall expire ten (10) years after the commencement of the Phase II Hotel Occupancy Period (Initial Term).

Following the Initial Term, Developer shall have two (2) 10-year options to extend the Term, exercisable by giving notice pursuant to Section 12 herein to City no later than ninety (90) days prior to the expiration of the Initial Term or extended Term as the case may be, with the number of permits to be issued to Hotel during any of the two option terms determined as outlined in Section 3.C below.

SECTION 3. **USE OF PARKING FACILITY**

- A. Parking Use. The Parking Permits issued by City to Developer under this Agreement are a non-exclusive license to park only, and shall be used solely by Developer for the parking of motor vehicles by Hotel guests in the Parking Facility during the Term of the Agreement and under the terms and conditions herein. Permit Holders may park in any parking spaces within the Parking Facility which are opened to Parking Permit holders.
- B. Number of Parking Permits. City will provide the Developer Parking Permits in amounts specified by Developer from time to time, up to a total of **Forty-One (41)** Parking Permits during the Phase II Hotel Occupancy Period of this Agreement. Hotel shall provide City notification no later than November 1st of each year specifying the number of Permits for the upcoming year.
- C. Number of Permits in Option Years. During any of the subsequent option terms outlined in Section 2 above, the number of permits issued to Hotel by City shall be determined by the average utilization of Parking Permits over the prior year, and consistent with City parking codes in effect at time of extension. For example, if during year 10 of the Initial Term of Phase II, the average utilization of the 41 Parking Permits issued to Hotel for that year is 35, then the number of Parking Permits to be issued for the upcoming option year may not exceed 35, so long as that number does not conflict with any City parking codes in effect at that time. During any option year, and based on mutual consent, the number of Parking Permits may be increased back up to no more than 41, based on demonstrated need and occupancy data supporting the request.

- D. Valet Operations. City may, at its sole discretion, institute a stack/valet parking operation at the Parking Facility. A stack/valet operation may require the Permit Holder to leave the key with the Operator for each motor vehicle that parks in the Parking Facility and the Operator shall have the right to park and move the vehicle as part of the stack/valet operation. The Developer or Hotel, including all Permit Holders, under this Agreement shall be obligated to participate in a stack/valet parking operation in the event the City institutes such operation.
- E. Programmed Upgrades. City shall have the right, at its sole discretion, to make any regular programmed repairs, upgrades, or improvements to the Parking Facility during the Term of this Agreement. If the City determines any repairs, upgrades or improvements are warranted to the Parking Facility that may impact the use of the Parking Facility by Developer or Hotel Permit Holders, City will provide written notice describing (i) the anticipated repairs, upgrade, or improvements to be completed at the Parking Facility, (ii) the anticipated length of the work, and (iii) the temporary relocation of the Permit Holders to another available parking facility located within 0.5 miles of the intersection of Market Street and W. Santa Clara Street located in Downtown San José, as depicted in Exhibit C, for the duration of the work. City shall provide at least sixty (60) days advance written notice of relocation of any Permit Holders pursuant to this Section 3.F. The Permit Holders shall be required to temporarily park in the designated parking facility for the length of time determined by the City is necessary for the City to make or cause to make any repairs, upgrades, or improvements, and under the same terms and conditions of this Agreement.
- F. Redevelopment/Reconstruction of Parking Facility. City shall have the right, at its sole discretion, to redevelop, reconstruct, or otherwise improve the Parking Facility at any time during the Term of this Agreement. If the City determines, in its sole discretion, to redevelop, reconstruct or otherwise improve the Parking Facility which would prohibit use of the Parking Facility by Permit Holders, City will provide written notice to Developer describing (i) the anticipated redevelopment, reconstruction or improvements to be completed at the Parking Facility site, (ii) the anticipated length of the work, and (iii) the temporary or permanent relocation of the Permit Holders to another available parking facility located within 0.5 miles of the intersection of Market Street and W. Santa Clara Street located in Downtown San José, as depicted in Exhibit C. City shall provide at least one (1) year advance written notice of relocation of any Permit Holders pursuant to this Section 3.E. The Permit Holders shall be required to temporarily or permanently park in the designated parking facility for the length of time or remaining term of the agreement as determined by the City is necessary for the redevelopment, reconstruction, or improvements, and under the same terms and conditions of this Agreement.
- G. Emergency Repairs. City shall also have the right, at its sole discretion to make any emergency repairs, emergency upgrades, or emergency improvements to the Parking Facility during the Term of this Agreement without any prior notice to Developer. If City determines, in its sole discretion, any emergency repairs, emergency upgrades or emergency improvements to the Parking Facility is required which would prohibit use of the Parking Facility by Permit Holders, the Permit Holders may be required to temporarily park in another designated parking facility within .5 mile of the intersection of Market Street and W. Santa Clara Street located in Downtown San José, as depicted in Exhibit C, for the length of time determined by the City is necessary for the City to make or cause to make any emergency repairs, emergency upgrades, or emergency improvements, and under the same terms and conditions of this Agreement.

SECTION 4. PARKING RATES

A. PHASE I DEVELOPMENT HOLDING PERIOD

Subject to the terms and conditions herein, Developer is required to make payments to City on an annual basis based on the Monthly Rate multiplied by the forty-one (41) Parking Permits which will be issued during Phase II Hotel Occupancy Period. The maximum period of Phase I shall be sixty (60) months from the commencement date of the Phase I Development Holding Period. For example, if the Phase I Development Holding Period commences on January 1, 2020, Developer shall pay City the Monthly Rate x 41 (permits) x 12 months for the period of January 1, 2020 to December 31, 2020.

If the City issues Developer the Certificate of Occupancy following January of any subsequent year during the Phase I period, the City shall apply the pro-rated amount paid for that year toward the amount due during the Phase II Hotel Occupancy Period outlined below.

B. PHASE II HOTEL OCCUPANCY PERIOD

Developer shall notify the City's Department of Transportation within five (5) business days upon receipt of the Hotels Certificate of Occupancy which shall trigger the execution of the Confirmation of Phase II Hotel Occupancy Period (Exhibit B). Developer shall pay City in advance on an annual basis the full value of each Parking Permit issued by City to Developer under this Agreement at the Monthly Rate, set forth below, as it may be adjusted as specified in this Agreement. The initial Phase II payment shall be made prior to the first day of the Phase II Hotel Occupancy Period pro-rated through December of that calendar year. Ongoing payments shall be made before the first day in January during the Phase II Hotel Occupancy Period. Payments shall be made by wire transfer to a designated City bank account or in the form of a check, cashier's check, or other immediately available fund sent to the City at the address set forth in the Notice provision below.

C. MONTHLY RATE

Developer shall pay to the City on an annual basis under this Agreement during the Phase I Period and the Phase II Period at the following rates:

1. Commencement of Phase I Development Holding Period:

The annual rate shall be computed by taking the then current Monthly Rate for parking in the Market San Pedro Street Garage as outlined in the City's Master Parking Rate Schedule and effective on January 1st of each year. At the commencement of the Phase I Development Holding Period and annually thereafter, the City will review the Master Parking Rate Schedule and determine the annual rate and notify the Developer by providing an invoice to Developers attention as outlined in Section 12 (Notices).

2. Commencement of the Phase II Hotel Occupancy Period:

During the initial ten (10) year period of the Phase II Hotel Occupancy Period, and each of the subsequent ten (10) year option periods thereafter during the Term of this Agreement, the Monthly Rate to be paid by Developer for each Parking Permit shall be the greater of the following rates:

- a. The base Monthly Rate shall be one hundred fifty dollars (\$150.00) and shall be increased annually by 3.5% to compute the annual fee. See Exhibit D, or
- b. The Monthly Rate shall be equivalent to the regular monthly rate set forth in the City's Master Parking Rate Schedule for the Parking Facility as established by the San José City Council, as it may be adjusted over time. Under this option, Developer would pay the City for each Parking Permit based on the regular monthly rate set forth by the City Council for the Parking Facility, and then increased annually by 3.5%.

3. City shall send a written notice and invoice to Developer by November 15th of each year of the Phase II Hotel Occupancy period setting forth the calculations of the monthly rates under Sections 4.C.2(a) and 4.C.2(b) above and informing Developer of the calculations to be used to set the Monthly Rate for the applicable year.

D. LATE CHARGES

Developer recognizes that late payment of all monies due under this Agreement will result in administrative and other additional expenses to City, the extent of which additional expense is extremely difficult and economically impractical to ascertain. Developer therefore agrees that if payment due hereunder from Developer to City remains unpaid fifteen (15) days after payment is due, Developer shall pay to City a one-time late charge equal to five percent (5%) of the delinquent amount owed to City. Developer agrees that such amount is a reasonable estimate of the loss and expense to be suffered by City as a result of such late payment by Developer and may be charged by City to defray such loss and expense. The provisions of this Section in no way relieve Developer of the obligation to pay City any amount on or before the date on which they are due, nor do the terms of this Section in any way affect City's remedies under this Agreement in the event any amount is unpaid after the due date.

SECTION 5. TERMINATION

- A. Mutual Consent. The Parties may terminate this Agreement at any time by mutual written consent, provided such termination is consistent with the Conditions of Approval for the Hotel. Unless the Parties agree otherwise, the termination shall become effective ninety (90) days after the written agreement to mutually terminate.
- B. Force Majeure. In the event that the Parking Facility is damaged by a Force Majeure Event rendering the Parking Facility totally inaccessible or unusable or more than fifty percent (50%) of the parking spaces dedicated for monthly parking are inaccessible or unusable, as determined by City during the Term of this Agreement for a period of at least one year, City may terminate this Agreement by giving Developer seven (7) days written notice thereof. In

the event of termination by City under this paragraph 5.B., Developer shall be refunded all Parking Permit fees and deposits paid in advance, subtracting any fees or charges owed to City, for each day that the Parking Facility was inaccessible or unusable before the date of termination. If said spaces are anticipated to be totally inaccessible or unusable for less than one year, City shall provide for the temporary or permanent relocation of the Permit Holders to another available parking facility located within 0.5 miles of the intersection of Market Street and W. Santa Clara Street located in Downtown San Jose, as depicted in Exhibit C.

- C. Taking by Eminent Domain. If the real property where the Parking Facility is located is partially or wholly taken by eminent domain, or is the subject of a pending taking which has not been consummated, City shall immediately notify Developer in writing of the event. In this event, this Agreement shall be terminated not sooner than forty-five (45) days prior to consummation of the taking. On termination of this Agreement under this Section 5.C., neither party shall have any rights or responsibilities to the other as of the date of termination, except for those obligations that incurred prior to the date of termination or survive termination.
- D. For Cause. In the event that either of the Parties fails to perform any terms, conditions, or obligations under this Agreement, in addition to all other remedies provided by law, either Party may terminate this Agreement for cause upon written notice of not less than forty-five (45) days.

SECTION 6. PARKING RULES AND REGULATIONS

In its use of the Parking Facility, Developer shall cause its Permit Holders to comply, with any rules and regulations established by City (“Rules”). City may modify or amend such Rules from time to time and Developer shall not have any right to approve or consent to any change in the Rules.

SECTION 7. INDEMNITY AND HOLD HARMLESS

Developer shall defend, indemnify and hold harmless City, its officers, employees and agents against any claim, loss or liability arising from or as a result of the death of any person or any accident, injury, loss or damage caused to any person or to the property of any person which shall occur on the Parking Facilities due to the willful or negligent acts (active to passive) or omissions by Developer, its officers, employees or agents, under this Agreement. This indemnity provision shall survive the expiration or sooner termination of this Agreement.

SECTION 8. WAIVER

City's failure, or Developer's failure, to enforce any breach of any term, covenant or condition contained herein shall not be deemed to be a waiver of such terms, covenants or conditions, or any subsequent breach of same, or any other term, covenant or condition contained herein.

SECTION 9. ASSIGNMENT OR TRANSFER

This Agreement, including any rights and/or obligations hereunder, may not be assigned or transferred in whole or in part by Developer during the Phase I Development Holding Period except by prior written consent of the City, which shall not be unreasonably withheld. This

SECTION 13. **NONDISCRIMINATION**

Neither City nor Developer shall discriminate, in any way, against any person on the basis of race, sex, color, age, religion, sexual orientation, actual or perceived gender identity, disability, ethnicity, or national origin, in connection with or related to the performance of this Agreement.

SECTION 14. **GIFTS**

- A. The Parties acknowledge the prohibition against the acceptance of any gift by a City officer or designated employee, and the prohibition against the acceptance of any gift by judicial officers, executives, or employees.
- B. The Parties agree not to offer any gift as prohibited.

SECTION 15. **CAPTIONS**

The article and paragraph captions contained in this Agreement are for convenience only and shall not be considered in the construction or interpretation of any provision hereof.

SECTION 16. **TIME OF ESSENCE**

Time is of the essence with respect to the performance of each and every provision of this Agreement.

SECTION 17. **SEVERABILITY**

If one or more of the provisions contained herein is for any reason held to be invalid, illegal or unenforceable in any respect, such invalidity, illegality or unenforceability shall not affect any other provision of this Agreement.

SECTION 18. **VENUE**

In the event that suit shall be brought by either Party to this Agreement, the Parties agree that venue shall be exclusively vested in the state courts of the County of Santa Clara or if federal jurisdiction is appropriate, exclusively in a United States District Court for the Northern District of California, San José, California.

SECTION 19. **ENTIRE AGREEMENT AND AMENDMENTS**

This Agreement contains all of the agreements of the Parties hereto with respect to any matter covered or mentioned in this Agreement, and no other agreement or understanding pertaining to any such matter shall be effective for any purpose. No provision of this Agreement may be amended or added, except by an agreement in writing signed by the Parties.

SECTION 20. **FORCE MAJEURE**

- A. “Force Majeure Event” shall be defined as any matter or condition beyond the reasonable control of a Party, including war, public emergency or calamity, fire, earthquake, extraordinary inclement weather, Acts of God, strikes, labor disturbances or actions, civil

disturbances or riots, litigation brought by third parties against either the City or Developer or both, or any governmental order or law which causes an interruption in the operation of the Parking Facility.

- B. Should a Force Majeure Event prevent performance of this Agreement, in whole or in part, the Party affected by the Force Majeure Event shall be excused or performance under this Agreement shall be suspended to the extent commensurate with the Force Majeure Event; provided that the Party availing itself of this Section 20 shall notify the other Parties within ten (10) days of the affected Party’s knowledge of the commencement of the Force Majeure Event; and provided further that the time of suspension or excuse shall not extend beyond that reasonably necessitated by the Force Majeure Event.

SECTION 21. REPRESENTATION AND WARRANTY

The Parties represent and warrant that they have the power and authority to enter into this Agreement, and that the appropriate governing body and/or officers have approved such power and authority to enter into this Agreement and bind the Parties, that this Agreement shall be executed, delivered and performed pursuant to the power and authority conferred by the appropriate governing body, and that the individual(s) executing this Agreement is duly authorized to do so.

City makes no warranty or representation on the condition of the Parking Facility and whether any repairs, upgrades, or improvements may be required during the Term that may impact this Agreement. City is providing the parking spaces in the Parking Facility to Developer in an “As Is” physical condition. Subject to appropriation of the City Council and available funding, City will provide routine maintenance of the Parking Facility in the same manner as other comparable parking facilities in Downtown San José.

WITNESS THE EXECUTION HEREOF on the day and year first hereinabove written:

“CITY”

APPROVED AS TO FORM:

CITY OF SAN JOSE, a municipal corporation

JON CALEGARI
Deputy City Attorney

By _____
Name:
Title:

“DEVELOPER”

Almaden Corner, LLC

By _____

By _____

EXHIBIT A

**CONFIRMATION OF PHASE 1 DEVELOPMENT HOLDING PERIOD
UNDER PARKING AGREEMENT
BETWEEN THE CITY OF SAN JOSE
AND
ALMADEN CORNER, LLC
FOR THE USE OF THE MARKET/SAN PEDRO SQUARE GARAGE**

This Confirmation of the Phase 1 Development Holding Period is made and entered into as of _____, 20____, by and between the City of San José, a municipal corporation of the State of California (“City”), and Almaden Corner, LLC (“Developer”).

RECITAL

WHEREAS, City and Developer entered into an agreement on _____ entitled “Parking Agreement Between the City of San José and Almaden Corner, LLC” (“Agreement”); and

WHEREAS, the Agreement sets forth the terms and conditions pursuant to which City will issue Parking Permits to Developer for use by the planned Hotel in the Market/San Pedro Square Garage, as defined in the Agreement; and

WHEREAS, the parties desire to enter into this Confirmation of Phase I Development Holding Period pursuant to the Agreement to set forth the commencement and expiration dates of the Phase I Development Holding Period;

The Parties agree as follows:

The **Phase I Development Holding Period**, as defined in the Agreement, commenced on _____ (*insert date*), and shall expire on _____ (*insert date*).

Almaden Corner, LLC

City of San Jose, a municipal corporation

By _____
Name:
Title:

By _____
Name:
Title:

EXHIBIT B

CONFIRMATION OF PHASE II HOTEL OCCUPANCY PERIOD

UNDER PARKING AGREEMENT

BETWEEN THE CITY OF SAN JOSE

AND

ALMADEN CORNER, LLC

FOR THE USE OF THE MARKET/SAN PEDRO SQUARE GARAGE

This Confirmation of Phase II Hotel Occupancy Period is made and entered into as of _____, 20____, by and between the City of San José, a municipal corporation of the State of California (“City”), and Almaden Corner, LLC (“Developer”).

RECITAL

WHEREAS, City and Developer entered into an agreement on _____ entitled “Parking Agreement Between the City of San José and Almaden Corner, LLC” (“Agreement”); and

WHEREAS, the Agreement sets forth the terms and conditions pursuant to which City will issue up to forty-one (41) parking permits to Developer for use by its planned Hotel to park in the Market/San Pedro Square Garage, as defined in the Agreement; and

WHEREAS, the parties desire to enter into this Confirmation of Phase II Hotel Occupancy Period pursuant to the Agreement to set forth the commencement and expiration dates of the Phase II Hotel Occupancy Period;

The Parties agree as follows:

The **Phase II Hotel Occupancy Period**, as defined in the Agreement, commenced on _____ (*insert date*), and shall continue through the remaining Term of the Agreement.

Almaden Corner, LLC

City of San Jose, a municipal corporation

By _____
Name:
Title:

By _____
Name:
Title:

EXHIBIT C

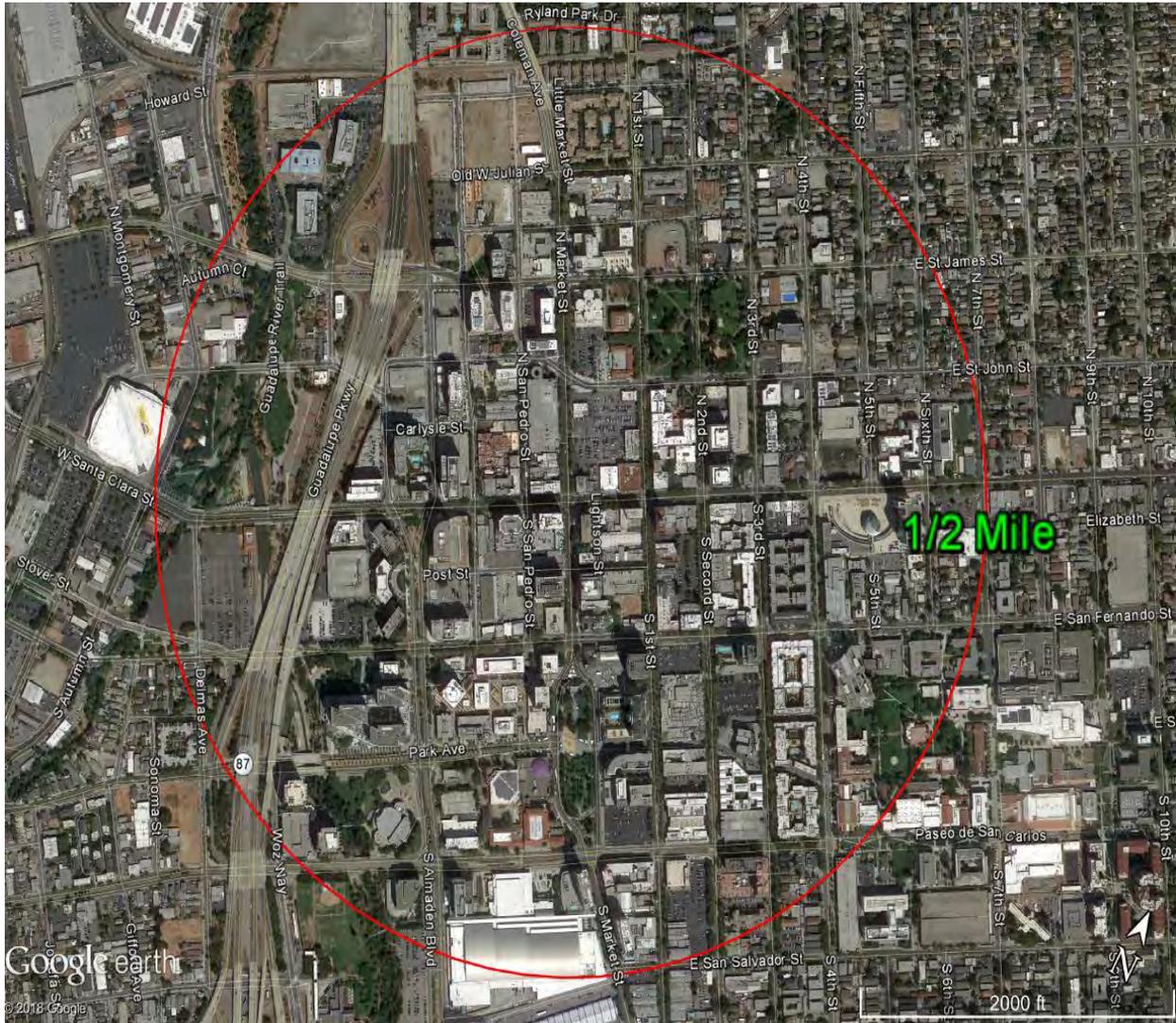


EXHIBIT D

**PHASE II HOTEL OCCUPANCY PERIOD
BASE ANNUAL FEE SCHEDULE**

Phase II Term	Monthly Rate	Permits	Annual Total
Year 1	\$ 150.00	41	\$ 73,800.00
Year 2	\$ 155.25	41	\$ 76,383.00
Year 3	\$ 160.68	41	\$ 79,056.41
Year 4	\$ 166.31	41	\$ 81,823.38
Year 5	\$ 172.13	41	\$ 84,687.20
Year 6	\$ 178.15	41	\$ 87,651.25
Year 7	\$ 184.39	41	\$ 90,719.04
Year 8	\$ 190.84	41	\$ 93,894.21
Year 9	\$ 197.52	41	\$ 97,180.51
Year 10	\$ 204.43	41	\$ 100,581.82
Year 11	\$ 211.59	41	\$ 104,102.19
Year 12	\$ 219.00	41	\$ 107,745.77
Year 13	\$ 226.66	41	\$ 111,516.87
Year 14	\$ 234.59	41	\$ 115,419.96
Year 15	\$ 242.80	41	\$ 119,459.66
Year 16	\$ 251.30	41	\$ 123,640.74
Year 17	\$ 260.10	41	\$ 127,968.17
Year 18	\$ 269.20	41	\$ 132,447.06
Year 19	\$ 278.62	41	\$ 137,082.70
Year 20	\$ 288.38	41	\$ 141,880.60
Year 21	\$ 298.47	41	\$ 146,846.42
Year 22	\$ 308.91	41	\$ 151,986.04
Year 23	\$ 319.73	41	\$ 157,305.55
Year 24	\$ 330.92	41	\$ 162,811.25
Year 25	\$ 342.50	41	\$ 168,509.64
Year 26	\$ 354.49	41	\$ 174,407.48
Year 27	\$ 366.89	41	\$ 180,511.74
Year 28	\$ 379.74	41	\$ 186,829.65
Year 29	\$ 393.03	41	\$ 193,368.69
Year 30	\$ 406.78	41	\$ 200,136.59

30 Year Total \$ 3,809,753.58

MEMORANDUM

From: Ben Huie, Kimley-Horn and Associates, Inc.

Frederik Venter, Kimley-Horn and Associates, Inc.

To: Mark Tersini, KT Urban

Date: June 21, 2019

Re: Proposed TDM Plan for Almaden Hotel in San Jose, CA

1. Introduction

Kimley-Horn and Associates, Inc. (Kimley-Horn) was retained by KT Urban to prepare a transportation demand management (TDM) plan for the proposed hotel project on the northeast corner of Almaden Boulevard and Santa Clara Street in the City of San Jose (City). The proposed hotel would consist of 272 guest rooms and a valet parking service with no on-site parking. The project is proposing a TDM plan to reduce the number of vehicle trips generated by the project and the parking spaces needed by the project. This memorandum describes the potential reductions in vehicle and parking demand due to location, adjacent uses, and infrastructure, and the proposed TDM measures to reduce the project's trip and parking generation.

2. Project Location

The proposed hotel would be located on the northeast corner of the intersection of Santa Clara Street and Almaden Boulevard, as shown in **Figure 1**.

Figure 1 – Project Location



Source: Google Maps

3. Existing Conditions

The following describes the existing transportation network near the project site including pedestrian, bicycle, transit facilities as well as existing public parking locations.

Existing Pedestrian Facilities

Sidewalks are mostly provided throughout the vicinity of the project site to allow pedestrians access to nearby transit stops, commercial uses, office uses and event centers. There are existing sidewalks and crosswalks present for the majority of the area surrounding the project along Santa Clara Street and Almaden Boulevard. Pedestrians can access the San Jose Diridon Station and the SAP Center to the west using sidewalks along Santa Clara Street. Access to the San Jose Convention Center to the south is provided by sidewalks located along Almaden Boulevard. Access to San Pedro Square Market is provided by sidewalks located along Santa Clara Street and San Pedro Street. Therefore, pedestrian facilities adjacent to the project site provides good connectivity to the major land uses surrounding the proposed project.

Existing Bicycle Facilities

Figure 2 shows existing bicycle facilities within the study area.

Class I bicycle paths are provided throughout the City of San Jose. The following is a list of the Class I bicycle paths near the study area:

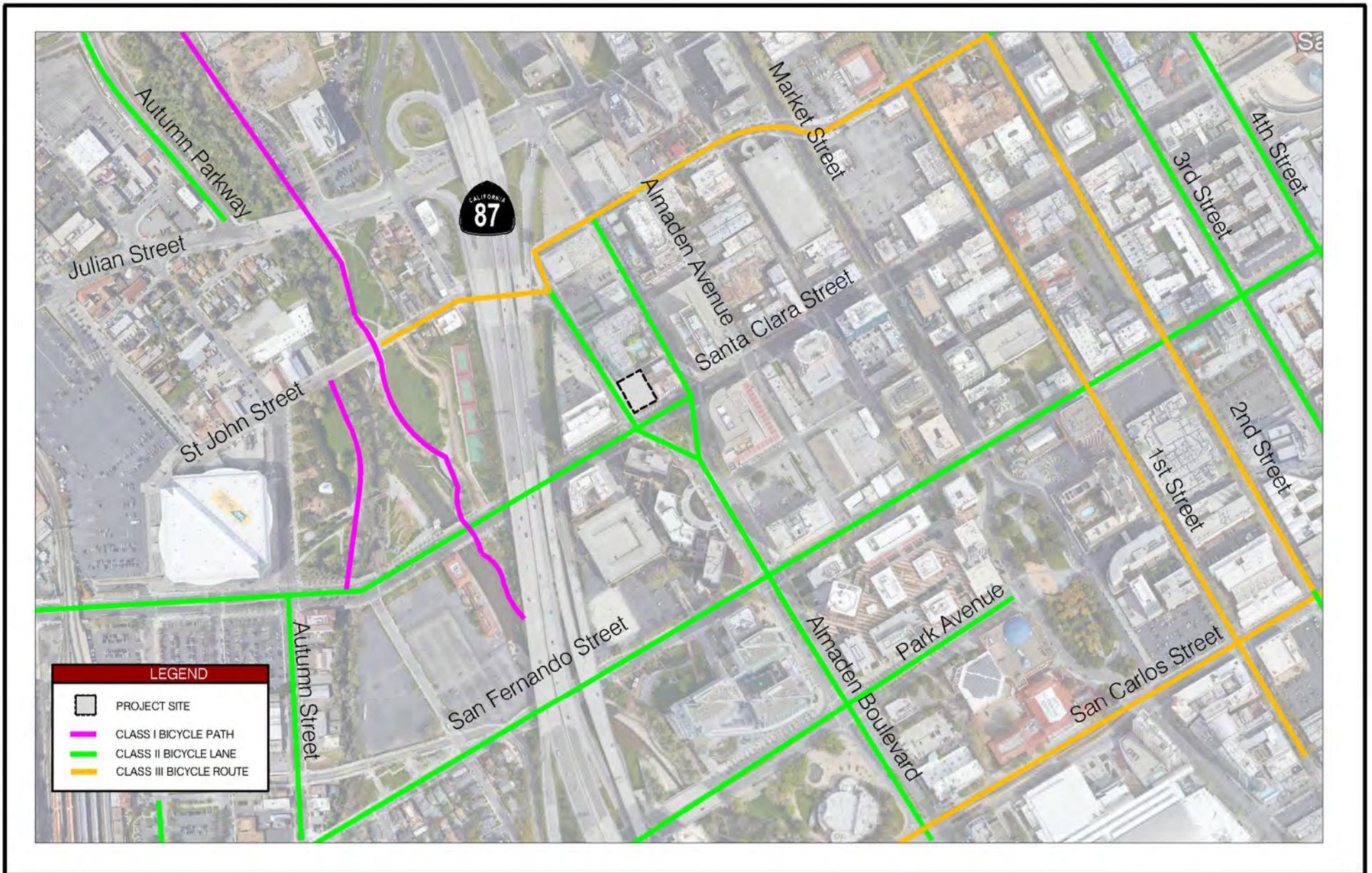
- Los Gatos Creek Trail between St John Street and Santa Clara Street
- Guadalupe River Trail between northern San Jose to south of Interstate-280

Class II bicycle lanes are provided throughout the City of San Jose. The following is a list of the Class II bicycle lanes near the study area:

- 3rd Street between Jackson Street and south of Keyes Street
- 4th Street between Jackson Street and Reed Street
- Almaden Boulevard between St John Street and Woz Way
- Autumn Parkway between Coleman Avenue and Julian Street
- Autumn Street between Santa Clara Street and Park Avenue
- Cahill Street between Crandall Street to San Fernando Street
- Montgomery Street between Park Avenue and San Carlos Street
- Notre Dame Avenue between St John Street and Santa Clara Street
- Park Avenue between Market Street and Bellomy Street
- Santa Clara Street between Notre Dame Avenue and Stockton Avenue
- San Fernando Street between Cahill Street and 11th Street

Class III bicycle routes are provided throughout the City of San Jose. The following is a list of the Class III bicycle routes near the study area

- 1st Street between John Street and San Salvador Street
- 2nd Street between John Street and San Carlos Street
- John Street between Almaden Boulevard and 2nd Street
- San Carlos Street between Woz Way and 4th Street



Existing Transit Facilities

Santa Clara Valley Transportation Authority (VTA), Caltrans, Monterey-Salinas Transit (MST), Altamont Corridor Express (ACE) and Amtrak provides service within the City of San Jose. The existing transit services within the study area are shown in **Figure 3** and described in this section.

VTA BUS SERVICES

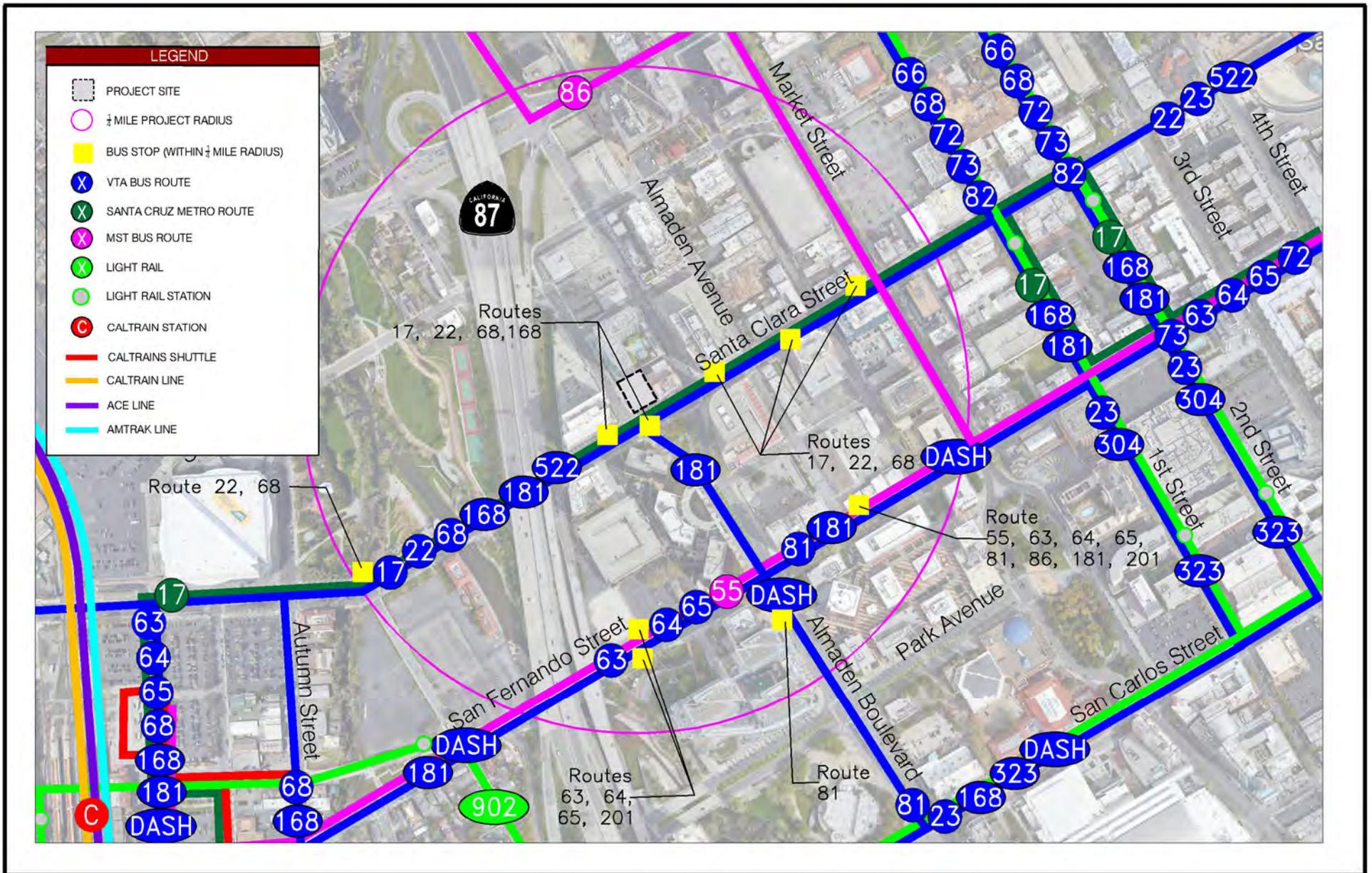
VTA has multiple bus routes near the project site and throughout the City of San Jose. Many routes (such as Route 23, 66, 72, etc.) operate near the project site but do not have bus stops located within walking distance. Therefore, only routes with bus stops located within walking distance ($\frac{1}{4}$ mile radius) of the proposed project area are described in this section.

Route 22 is a local bus service that operates between the Palo Alto Transit Center to the Eastridge Transit Center. In the vicinity of the project site, Route 22 runs on Santa Clara Street. On weekdays, Route 22 operates between 3:20 AM and 4:16 AM (of the following day) at approximately 15-minute to 80-minute headways. On Saturdays, Route 22 operates between 3:23 AM and 4:12 AM (of the following day) at approximately 15-minute to 80-minute headways. On Sundays, Route 22 operates between 3:21 AM and 4:09 AM (of the following day) at approximately 15-minute to 80-minute headways. Near the project site, there are bus stops along Santa Clara Street at Delmas Avenue, Almaden Boulevard, Almaden Avenue, and Market Street.

Route 63 is a local bus service that operates between San Jose State University to the intersection of Almaden Expressway and Camden Avenue in San Jose, CA. In the vicinity of the project site, Route 63 runs on San Fernando Street. On weekdays, Route 63 operates between 6:13 AM and 10:24 PM at approximately 30-minute to 120-minute headways. On Saturdays, Route 63 operates between 7:50 AM and 7:39 PM at approximately 60-minute headways. On Sundays, Route 63 operates between 8:48 AM and 5:38 PM at approximately 60-minute headways. Near the project site, there are bus stops along San Fernando Street at Almaden Boulevard and Almaden Avenue.

Route 64 is a local bus service that operates between the Almaden Light Rail Station to the intersection of McKee Road and White Road in San Jose, CA. In the vicinity of the project site, Route 64 runs on San Fernando Street. On weekdays, Route 64 operates between 5:22 AM and 11:22 PM at approximately 15-minute to 60-minute headways. On Saturdays, Route 64 operates between 6:26 AM and 11:04 PM at approximately 20-minute and 60-minute headways. On Sundays, Route 64 operates between 6:51 AM and 10:03 PM at approximately 30-minute and 60-minute headways. Near the project site, there are bus stops along San Fernando Street at Almaden Boulevard and Almaden Avenue.

Route 65 is a local bus service that operates between the intersection of Kooser Road and Blossom Hill Road and the intersection of Hedding Street and 13th Street in San Jose, CA. In the vicinity of the project site, Route 65 runs on San Fernando Street. On weekdays, Route 65 operates between 5:45 AM and 7:57 PM at approximately 45-minute and 60-minute headways. Route 65 does not operate on the weekends. Near the project site, there are bus stops along San Fernando Street at Almaden Boulevard and Almaden Avenue.



Route 68 is a local bus service that operates between the Gilroy Transit Center and the San Jose Diridon Transit Center. In the vicinity of the project site, Route 68 runs on Santa Clara Street. On weekdays, Route 68 operates between 4:00 AM and 1:26 AM (of the following day) at approximately 15-minute and 60-minute headways. On Saturdays, Route 68 operates between 5:44 AM and 1:31 AM (of the following day) at approximately 20-minute and 60-minute headways. On Sundays, Route 68 operates between 5:44 AM and 1:20 AM (of the following day) at approximately 20-minute and 60-minute headways. Near the project, there are bus stops along Santa Clara Street at Delmas Avenue, Almaden Boulevard, Almaden Avenue, and Market Street.

Route 81 is a local bus service that operates between the intersection of Akron Road and Mc Cord Avenue in Mountain View, CA and San Jose State University. In the vicinity of the project site, Route 81 runs on San Fernando Street. On weekdays, Route 81 operates between 6:06 AM and 9:04 PM at approximately 20-minute to 70-minute headways. On Saturdays, Route 81 operates between 9:12 AM and 6:14 PM at approximately 60-minute headways. Route 81 does not operate on Sundays. Near the project site, there are bus stops along San Fernando Street at Almaden Avenue and Almaden Boulevard.

Route 168 is a local bus service that operates between the Gilroy Transit Center and the San Jose Diridon Transit Center. In the vicinity of the project site, Route 168 runs on Santa Clara Street. On weekdays, Route 168 operates in the AM peak period between 5:32 AM and 8:55 AM in the northbound direction and in the PM peak period between 3:40 PM and 7:07 PM in the southbound direction at approximately 15-minute to 35-minute headways. Route 168 does not operate on weekends. Near the project, there is a bus stop at the intersection of Santa Clara Street and Almaden Boulevard.

Route 181 is a local bus service that operates between the Warm Springs Bart Station and the San Jose Diridon Transit Center. In the vicinity of the project site, Route 181 runs on San Fernando Street and Almaden Boulevard. On weekdays, Route 181 operates between 5:38 AM and 12:43 AM (of the following day) at approximately 15-minute to 60-minute headways. On Saturdays, Route 181 operates between 6:45 AM and 12:39 AM (of the following day) at approximately 20-minute to 45-minute headways. On Sundays, Route 181 operates between 7:17 AM and 12:44 AM (of the following day) at approximately 20-minute to 60-minute headways. Near the project, there is a bus stop at the intersection of San Fernando Street and Almaden Avenue.

Route 201 is the Downtown Area Shuttle (DASH) that operates between the San Jose Diridon Transit Center and San Jose State University. On weekdays, Route 201 operates between 6:35 AM and 9:28 PM at approximately 5- to 30-minute headways. Route 201 does not operate on weekends. In the vicinity of the project, Route 201 runs on San Fernando Street and there are bus stops along San Fernando Street at Almaden Boulevard and Almaden Avenue.

VTA LIGHT RAIL TRANSIT (LRT)

Line 902 is the Mountain View–Winchester Avenue light rail train (LRT) which operates between Downtown Mountain View and Winchester Avenue in Campbell. The closest LRT stations to the project site are the San Fernando and Santa Clara Stations. Both stations are within ½ mile of the project site. The San Fernando Station is located to the west while the Santa Clara Station is located to the east of the project site. On weekdays, Line 902 operates between 4:43 AM and 12:41 AM (of the following day) at approximately 15-minute to 30-minute headways. On Saturdays and Sundays, Line 902 operates between 6:00 AM and 12:44 AM (of the following day) at approximately 30-minute headways.

SAN CRUZ METRO

Highway 17 Express is an Amtrak Thruway bus that provides service between the Santa Cruz Metro Center and San Jose State University. In the vicinity of the project site, Highway 17 Express runs on Santa Clara Street. On weekdays, Highway 17 Expressway operates between 4:40 AM to 11:41 PM at approximately 20-minute to 40-minute headways. On weekends, Highway 17 Expressway operates between 6:52 AM to 10:57 PM at approximately 30-minute to 60-minute headways. Near the project site, there are bus stops along Santa Clara Street at Almaden Boulevard, Almaden Avenue, and Market Street.

MST BUS SERVICES

Route 55 is an Amtrak Thruway express bus that provides service between the San Jose Diridon Transit Station and the City of Monterey. In the vicinity of the project site, Route 55 runs on San Fernando Street. On weekdays, Route 55 operates between 8:29 AM to 5:20 PM. On weekends, Route 55 operates between 9:55 AM to 9:15 PM. There is a bus stop located at the intersection of San Fernando Street and Almaden Avenue near the project site.

Route 86 is an Amtrak Thruway bus that provides service between the San Jose Diridon Transit Station and the King City Mee Memorial Hospital. In the vicinity of the project site, Route 86 runs on San Fernando Street. On weekdays, Route 86 operates between 4:38 AM to 10:15 PM. On weekends, Route 86 operates between 6:05 AM to 9:55 PM. There is a bus stop located at the intersection of San Fernando Street and Almaden Avenue near the project site.

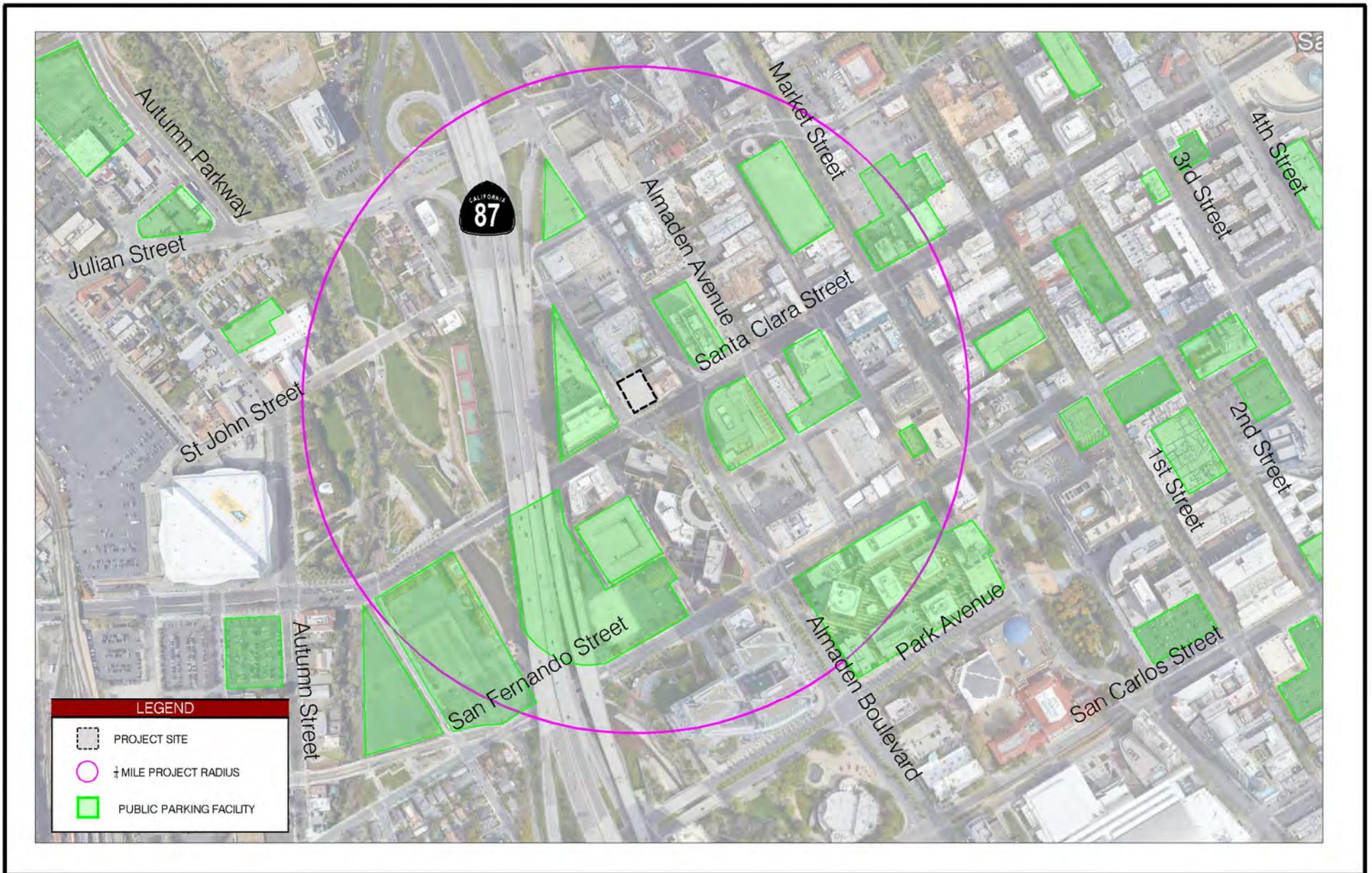
CALTRAIN

Caltrain provides commuter-heavy rail services between San Francisco County and Santa Clara County. The nearest Caltrain station to the project site is the Diridon Station located approximately 0.5 miles west of the project site. The current 2019 schedule shows that during the weekday AM peak, the Diridon Station is served by three northbound Baby Bullet trains, five northbound limited-stop trains, three southbound Baby Bullet trains, five southbound limited-stop trains, and one southbound local train. During the weekday PM peak, the station is served by four northbound Baby Bullet trains, six northbound limited-stop trains, two southbound Baby Bullet Trains, three southbound limited-stop trains, and one southbound local train. At the Diridon Station, there are connections to VTA bus routes 63, 64, 65, 68, 168, 181, and 201, MST routes 55 and 86, San Mateo Metro Highway 17 Express bus, and the Tamien/San Jose Diridon Shuttle.

The Tamein/San Jose Diridon Shuttle provides shuttle service between the Tamien Caltrain Station and the San Jose Diridon Caltrain Station. In the vicinity of the project, the shuttle operates along San Fernando Street. It operates only on weekends from 8:11 AM to 10:17 PM. The shuttle stops only at the San Jose Diridon and Tamien Caltrain station with no stops in between.

Existing Parking Locations

Existing parking facilities adjacent to the project site were identified and shown in **Figure 4**. As shown in **Figure 4**, public parking facilities are available within walking distance of the project site. Parking lots adjacent to the project sight include the Commonwealth/Comerica Garage, Almaden Financial Plaza, 10 Almaden Garage, and the 225 W Santa Clara Garage.



4. Proposed TDM Program Elements

The following summarizes an initial approach to the proposed TDM program for the proposed project. It is assumed that the TDM program will be refined over time to adapt to changing transportation trends and to maximize the efficiency of the program. The TDM program is specifically designed to focus on incentives for hotel employees and guests to participate in the program rather than penalties for not participating. An initial set of TDM measures are proposed for the Almaden Hotel project and is summarized in **Table 1**.

Table 1. Proposed TDM Measure Summary

TDM Measure	Description
Location-Based	
Proximity to Pedestrian Facilities	There exist sidewalks on the adjacent streets to the project (e.g. Almaden Boulevard, Santa Clara Street, and Notre Dame Avenue). In addition, there are crosswalks at the nearby intersections.
Proximity to Bicycle Facilities	There are existing bicycle facilities on the adjacent streets to the project (e.g. Almaden Boulevard, Santa Clara Street, and Notre Dame Avenue). See Figure 2 .
Proximity to Transit	There are existing bus routes operated by VTA (Routes 22, 63, 64, 65, 68, 81, 168, 181, and 201), MST (Routes 55 and 86), and Santa Cruz Metro (Highway 17 Express) that have bus stops within ¼-mile of the project. The bus stops are at the intersection of Almaden Boulevard and Santa Clara Street, midblock of San Fernando Street from Delmas Avenue to Almaden Boulevard, and at the intersection of Almaden Avenue and San Fernando Street. The San Jose Diridon Transit Station is located approximately ½ mile west of the project site. There are two light rail train (LRT) stops (Santa Clara Station along 2 nd Street and San Fernando Station along San Fernando Street) that is approximately ½ mile away. See Figure 3 .
Proximity to Complimentary Uses	The project is located in Downtown San Jose. There are multiple complimentary uses that are within a ½-mile radius of the site. These uses include restaurants and bars in San Pedro Square to the northeast, the SAP Center to the west, and San Jose Convention Centre to the south.
Site Design-Based	
Free VTA SmartPass for Employees	The proposed project will provide free VTA SmartPasses to all hotel employees.
Employee Parking Incentive	Employees will be incentivized to not park at the off-site parking lot. Employees will be given a financial incentive, yet to be determined, if they do not drive to work and park at the off-site parking lot.
On-Site TDM Coordinator	The proposed project will provide an on-site TDM coordinator who will implement and manage the TDM plan. On-site information with regards to public transit services, transit passes, bicycle maps, bike share information, rideshare/carpool program, Zipcar station locations, and ride matching services.

5. Estimated TDM Reductions

Table 2 estimates the trip reduction percentage of each TDM element. A literature review was performed for the potential TDM measures to determine a conservative trip reduction percentage associated with each trip reduction strategy. The total estimated trip reduction based on the proposed TDM measures can reach up to 7 percent. Since the TDM measures only focus on the employee trips, and the employee trips are much less than the hotel guest trips, the trip reductions shown are minimal. Also, please note that the estimated trip reductions are based on the City's VMT tool, which does not include a hotel use, but has a retail and office use. In addition, the reductions are shown as VMT reductions, and therefore we assumed the percent VMT reduction was equivalent to the percent trip reduction.

Table 2. Estimated Trip Reductions

TDM Measure	Estimated Trip Reduction (%)	Trip Reduction Source	Notes/ Assumptions
Free VTA SmartPass for Employees	3%	San Jose VMT tool	Assumes 25% reduction of employee trips Assumes employee peak hour trips are 10% of all trips
Employee Parking Incentive	4%	San Jose VMT tool	Assumes retail use and trip reduction is equivalent to VMT reduction
On-Site TDM Coordinator	0%	-	Combined with other TDM Programs, so no discount by itself
Estimated Total	7%		

6. TDM Monitoring

The City of San Jose does not require monitoring of the vehicle trips for TDM plans. However, to determine the effectiveness of the TDM program, it is recommended that annual monitoring be conducted to evaluate the TDM program participation and adjust the TDM program, if needed. The following monitoring program is recommended.

- **Annual Survey of Vehicle Trips:** An annual survey of the vehicle trips generated by the project should be conducted. The counts should be conducted on an annual basis to quantify the vehicle trips generated during the AM and PM peak hours. Counts should be collected when local schools are in session and when weather conditions are typically more moderate.
- **Recommendations:** Based on the surveys collected, recommendations will be summarized for changes to the TDM plan to increase effectiveness of the plan.
- **Compliance:** Since the project does not have a required TDM goal, there is no compliance requirements or penalties included in the TDM memorandum.
Report: The process, surveys, analysis, and recommendations will be summarized in a TDM Monitoring Report.

ALMADEN CORNER HOTEL

8 N ALMADEN BLVD SAN JOSE, CA
HOTEL MIXED-USE DEVELOPMENT

C2K

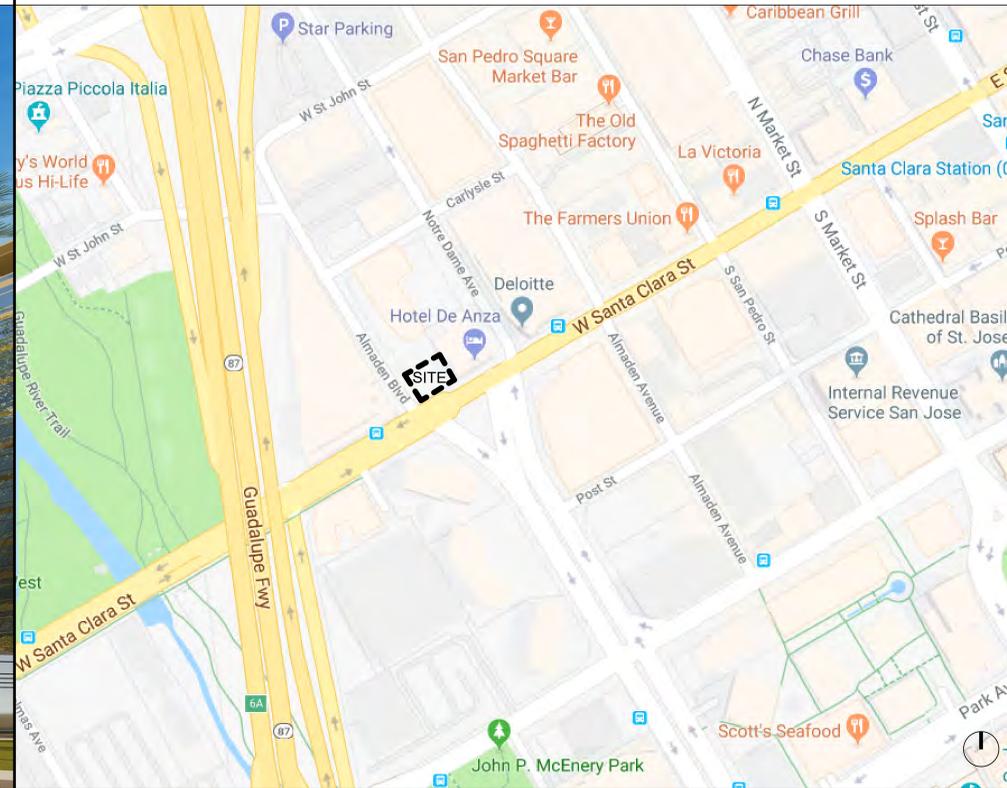
ARCHITECTURE

1645 NW HOYT
PORTLAND OREGON 97209
503 444 2200

NORTH VIEW AT ALMADEN & W. SANTA CLARA ST.



VICINITY MAP



PROJECT TEAM

OWNER / DEVELOPER

KT Urban
21710 Stevens Creek Blvd., Ste. 200
Cupertino, CA 95014
P: 408.267.2100
CONTACT:
Mark Tersini mtersini@aol.com

ARCHITECT

C2K ARCHITECTURE INC.
1645 NW Hoyt
Portland, OR 97209
P: 503.444.2200
CONTACT:
Kevin Sauser ksauser@c2karch.com
Nathan Miller nmiller@c2karch.com

GENERAL CONTRACTOR

CIVIL ENGINEER

Charles W. Davidson Co.
255 West Julian St. Ste. 200
San Jose, CA. 95110
P: 408.295.9162
CONTACT:
Peter Smith psmith@cwdsco.com

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
SAN JOSE, CA 95110
H18-038**

PROJECT NO.: 17121
DRAWN: PM
DATE: 11/21/2019
H18-038 SITE DEVELOPMENT PERMIT
REVISION: DESCRIPTION:

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PERMITS / REVIEWS

REVIEWING AGENCY	REVIEW STATUS	REVIEWING AGENCY	REVIEW STATUS
CITY OF SAN JOSE, CA		BUILDING DEPARTMENT	PC#
COMPREHENSIVE PRELIMINARY REVIEW			PERMIT #
SUBMITTED	4/23/2018		
FILE #	PRE18-076		
SITE DEVELOPMENT PERMIT		FEDERAL AVIATION ADMINISTRATION (FAA)	
SUBMITTED	08/28/2018, 01/30/2019, 05/13/2019		
FILE #	H18-038		
DIRECTOR'S ACTION			
DEPARTMENT OF PUBLIC WORKS			
PROJECT #			
PERMIT #			
GRADING & DRAINAGE PERMIT			
PW PROJECT #			
REVOCABLE ENCROACHMENT PERMIT			

PROJECT DESCRIPTION

THE PROPOSED ALMADEN CORNER HOTEL DEVELOPMENT IS A 19 STORY HIGH RISE TOWER CONTAINING 272 GUEST ROOMS IN DOWNTOWN SAN JOSE. THE BUILDING WILL BE A POST TENSIONED CONCRETE STRUCTURE WITH EXTERIOR CLAD IN A HIGH QUALITY GLAZING SYSTEM WITH VISION, SPANDREL, AND OPERABLE AWNING WINDOWS. AMENITIES INCLUDE A GROUND LEVEL LOBBY BAR, TWO MEETING ROOMS, A FITNESS CENTER, AND A ROOF TERRACE ON LEVEL 19 WITH BAR / RESTAURANT.

H18-038 SITE DEVELOPMENT PERMIT

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SHEET TITLE:
COVER SHEET

SHEET NO.:
G000



LOOKING NORTH ON ALMADEN BLVD.



LOOKING SOUTH ON ALMADEN BLVD.



LOOKING NORTHWEST ON W. SANTA CLARA STREET



LOOKING EAST ON W. SANTA CLARA STREET

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

PROJECT NO.: 17121
 DRAWN: BK
 DATE: 11/21/2019
 H18-038 SITE DEVELOPMENT PERMIT

REVISION: DESCRIPTION:

H18-038 SITE DEVELOPMENT PERMIT

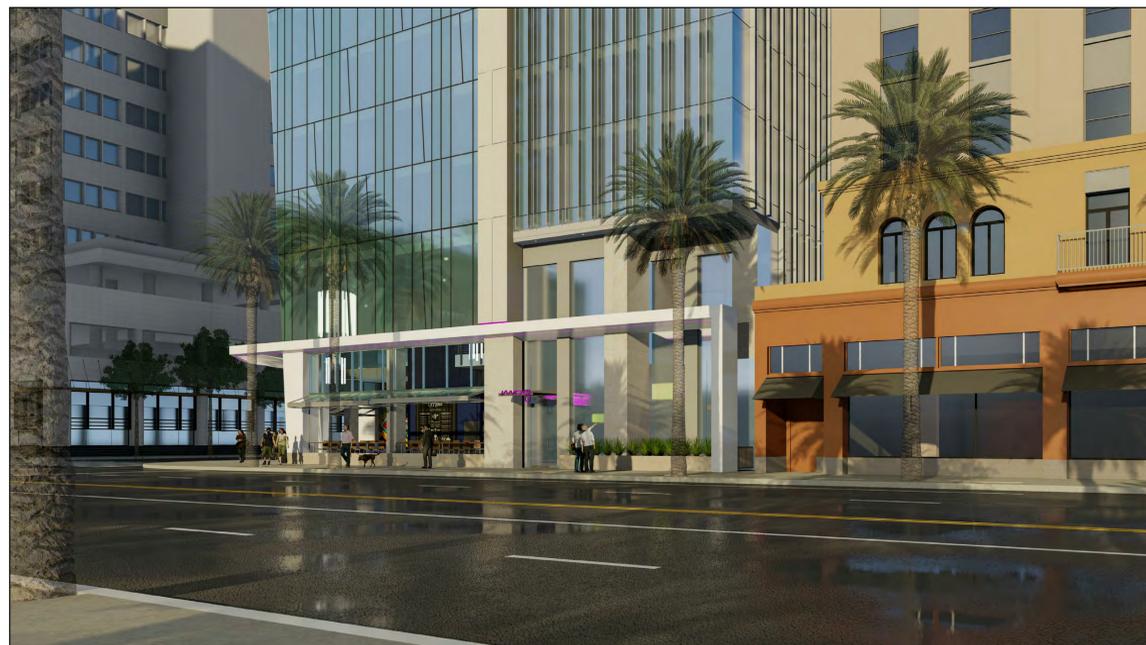
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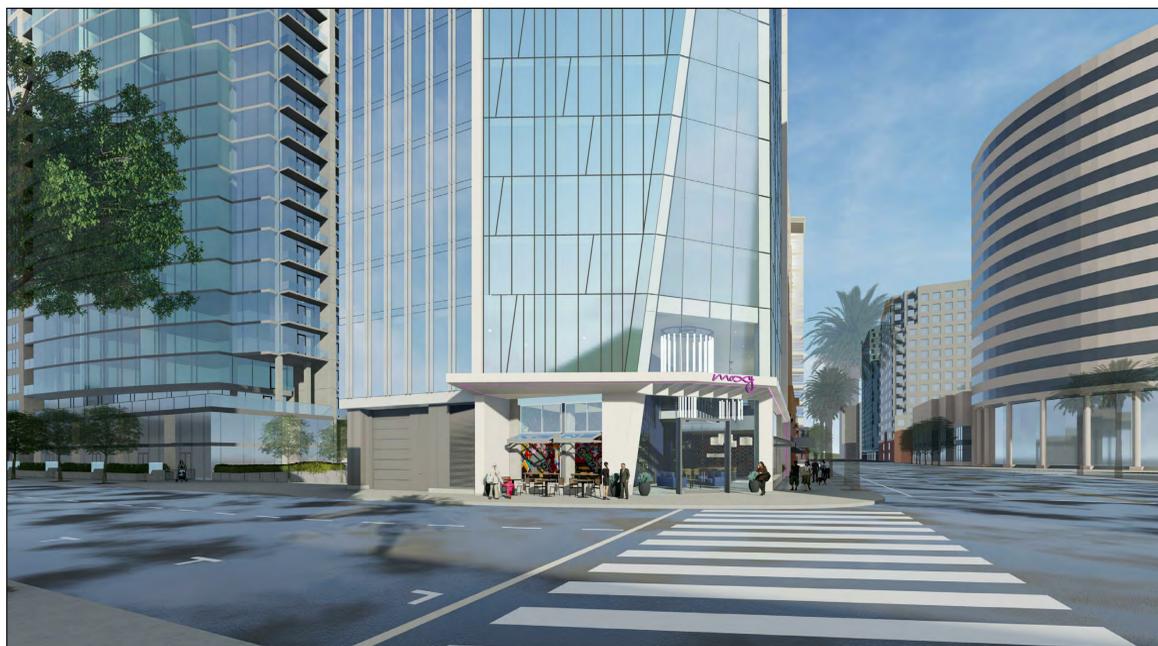
SHEET NO.:
G002



NORTH STREET VIEW AT CORNER OF N. ALMADEN BLVD AND W. SANTA CLARA STREET



NORTHWEST STREET VIEW ON W. SANTA CLARA STREET



EAST STREET PERSPECTIVE FROM N. ALMADEN BLVD.



SOUTHEAST STREET PERSPECTIVE FROM N. ALMADEN BLVD

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

PROJECT NO.: 17121
 DRAWN: BK
 DATE: 11/21/2019
 H18-038 SITE DEVELOPMENT PERMIT
 REVISION: DESCRIPTION:

H18-038 SITE DEVELOPMENT PERMIT

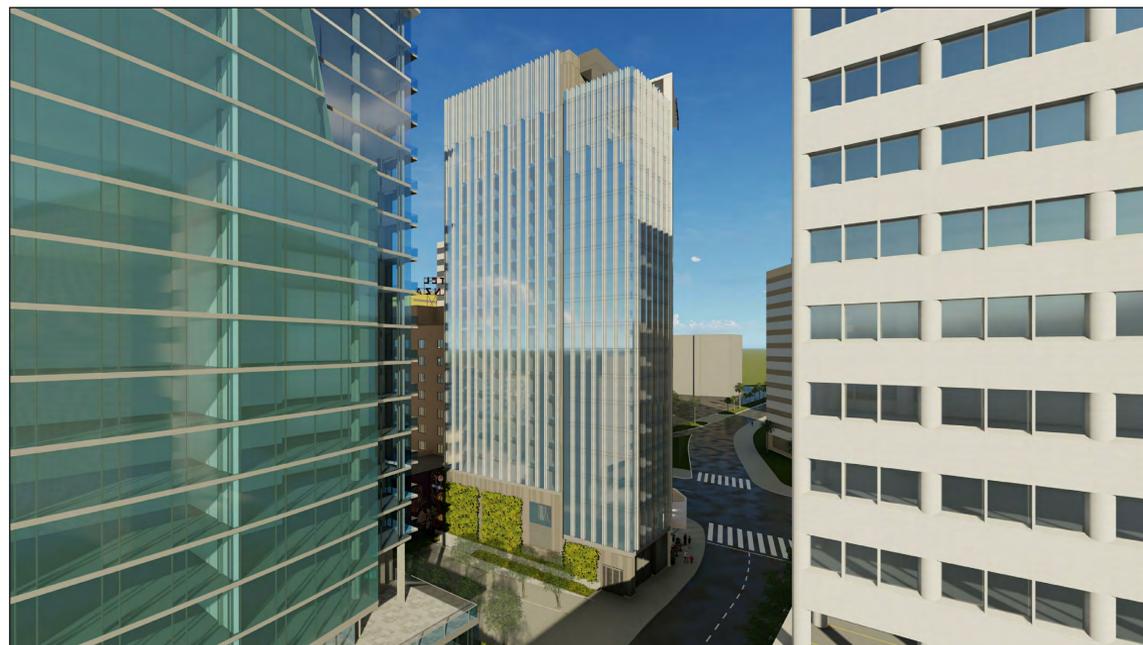
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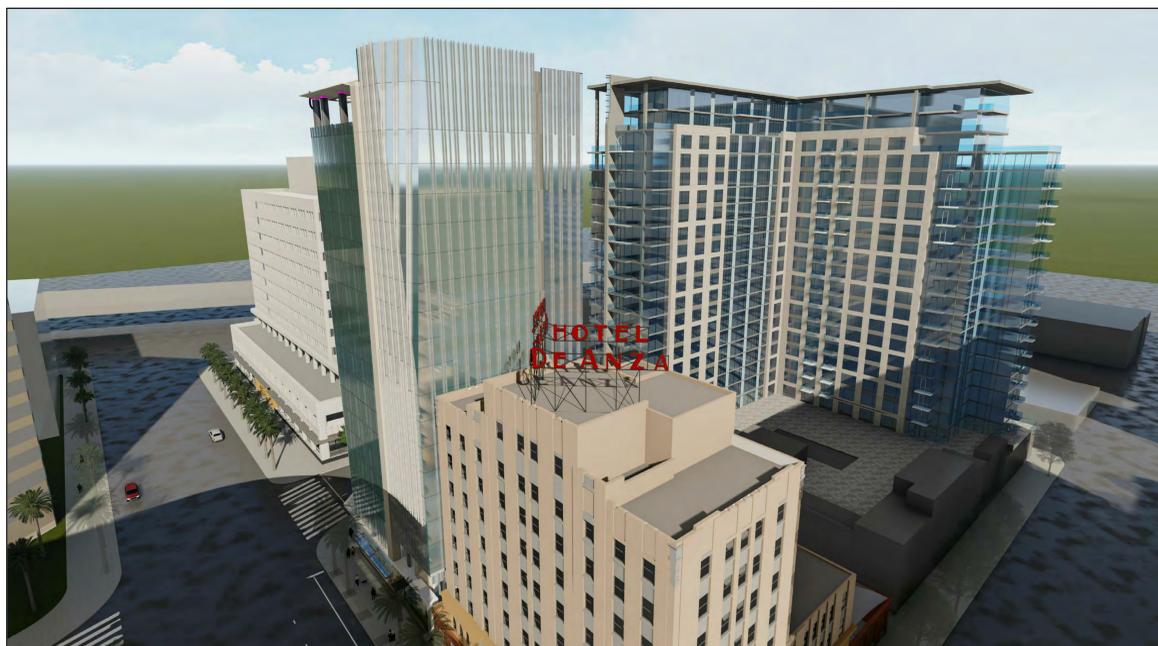
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G003



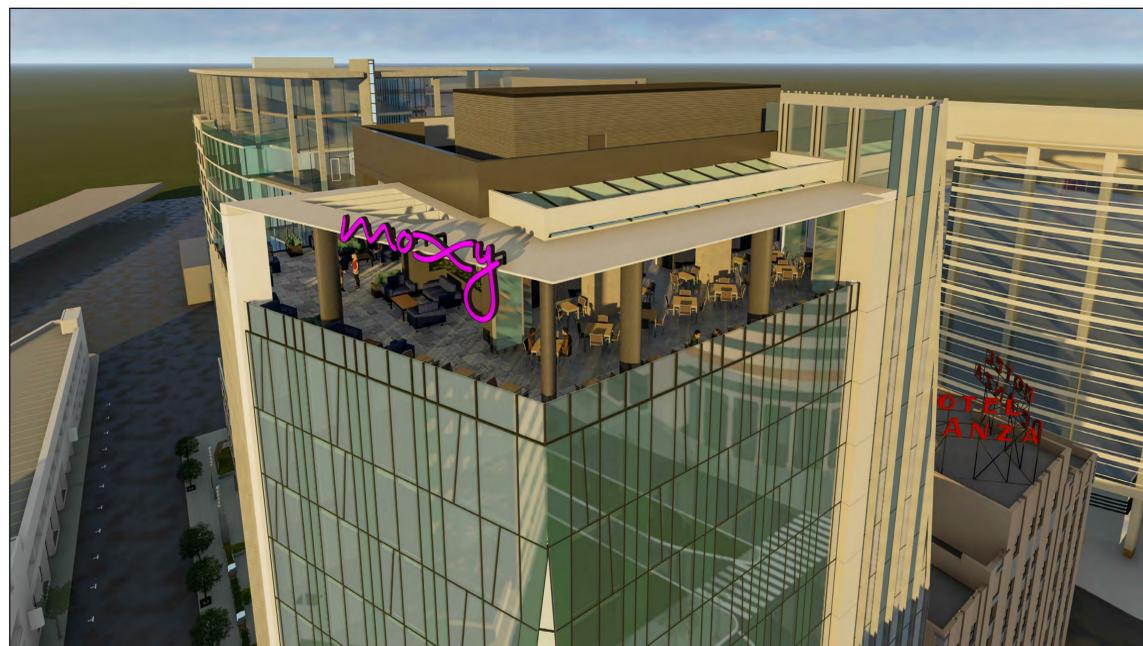
AERIAL LOOKING SOUTHWEST



AERIAL LOOKING SOUTHEAST



AERIAL LOOKING NORTHWEST



AERIAL LOOKING NORTHEAST AT ROOFTOP TERRACE

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

PROJECT NO.: 17121
 DRAWN: BK
 DATE: 11/21/2019
 H18-038 SITE DEVELOPMENT PERMIT

REVISION: DESCRIPTION:

H18-038 SITE DEVELOPMENT PERMIT

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SHEET TITLE:
PROJECT IMAGES

SHEET NO.:
G004



STREET PERSPECTIVE LOOKING NORTHEAST



VIEW LOOKING EAST DOWN W. SANTA CLARA ST.



PERSPECTIVE LOOKING NORTHEAST



PERSPECTIVE LOOKING NORTHWEST

**ALMADEN
CORNER
HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
SAN JOSE, CA 95110
H18-038**

PROJECT NO.: 17121
DRAWN: Author
DATE: 11/21/2019
H18-038 SITE DEVELOPMENT PERMIT

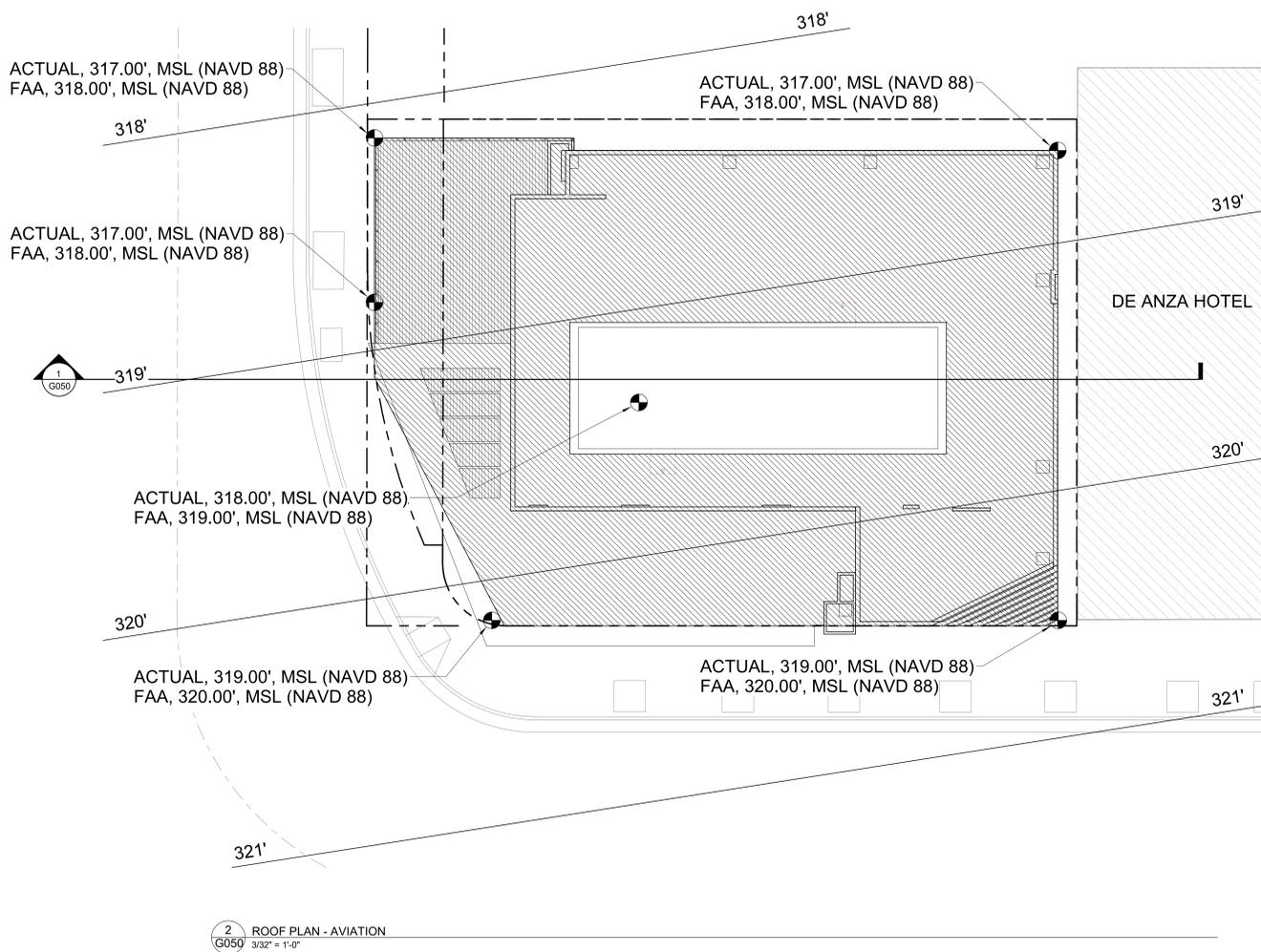
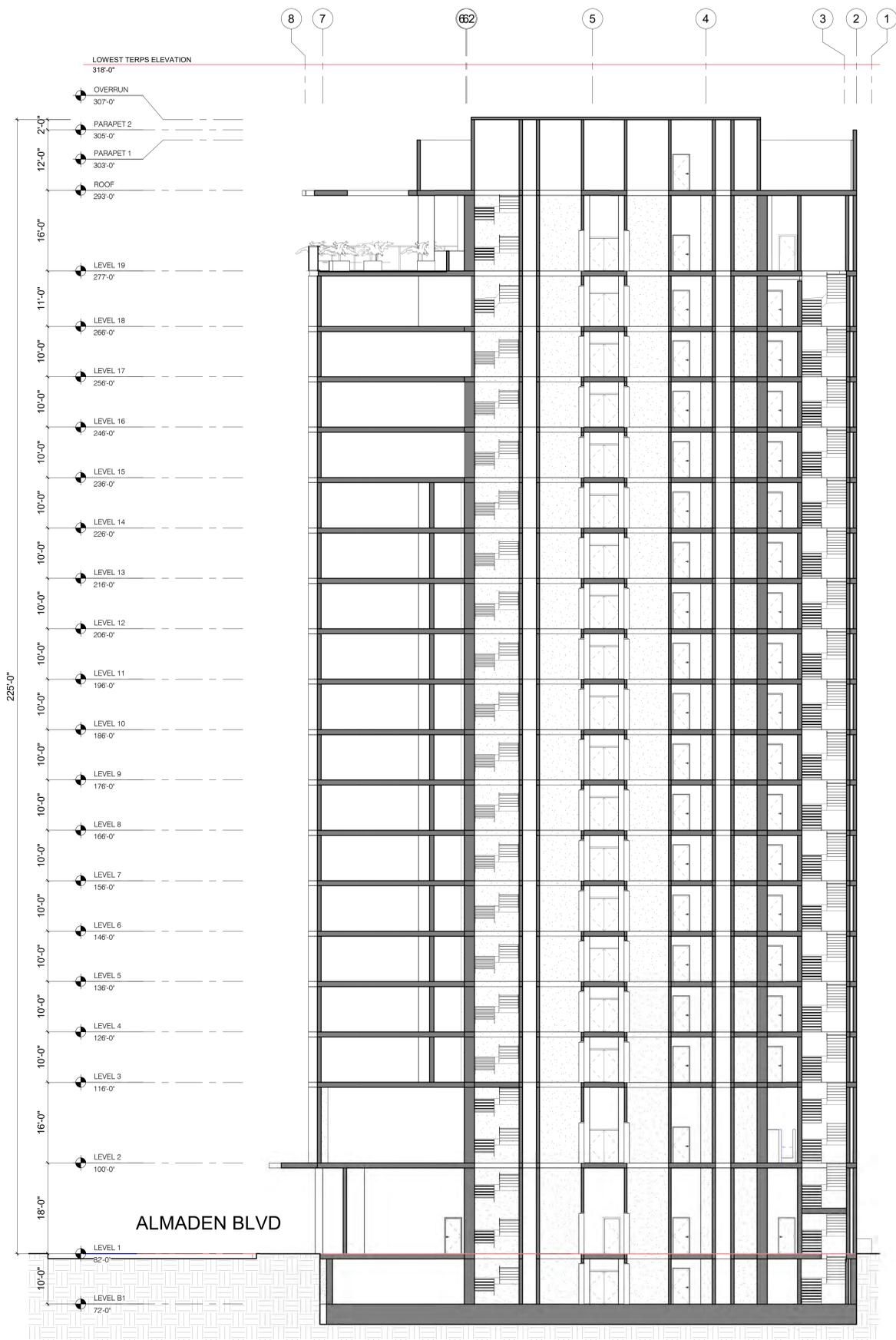
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H18-038 SITE DEVELOPMENT PERMIT

SHEET TITLE:
PROJECT IMAGES

SHEET NO.:
G005



**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

PROJECT NO.: 17121
 DRAWN: PM
 DATE: 11/21/2019
 H18-038 SITE DEVELOPMENT PERMIT

REVISION: DESCRIPTION:

H18-038 SITE DEVELOPMENT PERMIT

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SHEET TITLE:
AVIATION CLEARANCES

SHEET NO.:

G050

2 ROOF PLAN - AVIATION
 3/32" = 1'-0"

1 SITE SECTION NORTH - AVIATION
 3/32" = 1'-0"

BUILDING AREA SUMMARY

Almaden Corner Hotel - MOXY

	Gross Area (SF)	Number Rooms	Const. Type	Height (ft) Fir to Fir	Height To Floor Level
Roof	1,100	-	-	-	-
19th Bar	5,540	-	I-A	14.00	215.00
18th Guest Rooms	7,797	17	I-A	16.00	211.00
17th Guest Rooms	7,789	17	I-A	11.00	195.00
16th Guest Rooms	7,800	17	I-A	10.00	184.00
15th Guest Rooms	7,831	17	I-A	10.00	174.00
14th Guest Rooms	7,814	17	I-A	10.00	164.00
13th Guest Rooms	7,839	17	I-A	10.00	154.00
12th Guest Rooms	7,816	17	I-A	10.00	144.00
11th Guest Rooms	7,813	17	I-A	10.00	134.00
10th Guest Rooms	7,807	17	I-A	10.00	124.00
9th Guest Rooms	7,799	17	I-A	10.00	114.00
8th Guest Rooms	7,790	17	I-A	10.00	104.00
7th Guest Rooms	7,780	17	I-A	10.00	94.00
6th Guest Rooms	7,770	17	I-A	10.00	84.00
5th Guest Rooms	7,758	17	I-A	10.00	74.00
4th Guest Rooms	7,745	17	I-A	10.00	64.00
3rd Guest Rooms	7,732	17	I-A	10.00	54.00
2nd Library / Administration / Fitness	7,528	-	I-A	16.00	34.00
1st Lobby / Lounge / Bar / BOH	7,434	-	I-A	18.00	18.00
B1 Basement - BOH	7,915	-	I-A	-10.00	-10.00
Total	154,197	272		225.00	Total Building Height

Site Area	0.20 acres	8,756 SF
Proposed FAR Area	146,282	
Proposed FAR	16.7	

BUILDING CODE DATA

PROJECT INFORMATION

PROJECT NAME: Almaden Corner Hotel
ADDRESS: 8 N. Almaden Blvd
 San Jose, CA 95110
OWNER: KT Urban
 21710 Stevens Creek Blvd # 200
 Cupertino, CA 95014
ASSESSOR PARCEL#: 259-35-055
SEISMIC ZONE: DESIGN CATEGORY D

BUILDING NARRATIVE

272 GUEST ROOMS IN 19 STORY TOWER, INCLUDES GROUND LEVEL RESTAURANT/BAR, SECOND LEVEL MEETING AND FITNESS ROOMS, AND ROOF TERRACE WITH RESTAURANT/BAR.

APPLICABLE BUILDING REGULATIONS

- PART 1 - 2016 CALIFORNIA BUILDING STANDARDS ADMINISTRATIVE CODE
- PART 2 - 2016 CALIFORNIA BUILDING CODE (BASED ON 2015 IBC) WITH SAN JOSE CITY AMENDMENTS
- PART 3 - 2016 CALIFORNIA ELECTRICAL CODE (BASED ON 2014 NEC) WITH SAN JOSE CITY AMENDMENTS
- PART 4 - 2016 CALIFORNIA MECHANICAL CODE (BASED ON 2015 UMC) WITH SAN JOSE CITY AMENDMENTS
- PART 5 - 2016 CALIFORNIA PLUMBING CODE (BASED ON 2015 UPC) WITH SAN JOSE CITY AMENDMENTS
- PART 6 - 2016 CALIFORNIA ENERGY CODE
- PART 9 - 2016 CALIFORNIA FIRE CODE (BASED ON 2015 IFC)
- PART 11 - 2016 CALIFORNIA GREEN BUILDING STANDARDS CODE
- PART 12 - 2016 CALIFORNIA REFERENCE STANDARDS CODE
- ZONING ORDINANCE, CITY OF SAN JOSE, CALIFORNIA TITLE 20 OF SAN JOSE MUNICIPAL CODE

BUILDING OCCUPANCY AND SEPARATIONS CHAPTER 3

OCCUPANCY CLASSIFICATION GROUPS: A-2, A-3, B, M, R-1, S-1, S-2
OCCUPANCY SEPARATION (TABLE 508.4): NON SEPARATED PER 508.4

ALLOWABLE HEIGHT AND AREA CHAPTER 5

ALLOWABLE AREA PER FLOOR (TABLE 506.2): UNLIMITED
FRONTAGE INCREASE FACTORS

	NORTH	EAST	SOUTH	WEST
FRONTAGE (506.3)	3'-0"	3'-0"	87'-6"	60'-6"

FRONTAGE SHOWN BUT NOT USED FOR AREA INCREASE
 *NORTH SETBACK INCREASES TO 5'-0" ABOVE LEVEL 1

CONSTRUCTION TYPE: I-A

	ALLOWABLE	PROPOSED
BUILDING HEIGHT (PER TABLE 504.3)	UNLIMITED*	226'-0"
NUMBER OF STORIES (PER TABLE 504.4)	UNLIMITED*	19 STORIES*
AUTOMATIC SPRINKLER INCREASE	N/A	N/A

*SITE IS SUBJECT TO HEIGHT LIMITATIONS SET BY FAA FOR SAN JOSE AIRPORT OPERATIONS.

ZONING

LOCATION: APN 259-35-055
 Site bounded by N. Almaden Blvd, Santa Clara

ZONING DISTRICT	DC	Downtown Primary Commercial
LOT SIZE	8,756 SF	= 0.20 ACRES Gross

FAR BUILDABLE AREA	8,756 SF	LOT AREA	
AREA PROPOSED (FAR Gross)	= 145,225 SF		
145,225 SF / 8,756	= 16.6		
PROPOSED FAR	16.6:1		
CoSJ GENERAL PLAN 2040	FAR: Up to 30.0		

PARKING ANALYSIS

VEHICLE PARKING
 TABLE 20-190 REQUIREMENT: 0.35 SPACE PER GUEST ROOM

272 UNITS * 0.35 = 96 (20% reduction for downtown CoSJ projects)
77 SPACES REQUIRED
0 SPACES PROPOSED

*Valet parking will be provided at off-site garage

BICYCLE PARKING
 TABLE 20-190 REQUIREMENT: 1 SPACE PLUS 1 PER 10 GUEST ROOMS

272 UNITS / 10 = 27 + 1 = 28
28 SPACES REQUIRED
36 SPACES PROPOSED

LOADING SPACES
 20.70.435 REQUIREMENT: 1 SPACE (50,000-200,000 GSF)

VALET SPACES
 20.90.200 REQUIREMENT: 2 SPACES PROVIDED

BUILDING CONSTRUCTION CHAPTERS 6, & 7

FIRE RESISTANCE RATING REQUIREMENTS FOR BUILDING ELEMENTS (TABLE 601)

BUILDING ELEMENT	REQUIREMENT
STRUCTURAL FRAME	TYPE - IA
BEARING WALLS - EXTERIOR	3
BEARING WALLS - INTERIOR	2"
NON BEARING WALLS AND PARTITIONS	0
FLOOR CONSTRUCTION	2
ROOF CONSTRUCTION	1*

* REDUCTION IN RATING TO TYPE 1B REQUIREMENTS PER 403.2.1.1

FIRE RESISTANCE RATING REQUIREMENTS FOR EXTERIOR WALLS BASED ON FIRE SEPARATION

DISTANCE (TABLE 602)	NORTH	EAST	SOUTH	WEST
FIRE SEPARATION DISTANCE PROVIDED	5'-0"	3'-0"	44'-0"	30'-6"
TYPE IA - EXT WALL REQUIREMENTS - BEARING	PER TBL 601			
- NON BEARING	1-HR	1-HR	0-HR	0-HR

MAXIMUM AREA OF EXTERIOR WALL OPENINGS (TABLE 705.8)

	NORTH	EAST	SOUTH	WEST
FIRE SEPARATION DISTANCE PROVIDED	5'-0"	3'-0"	44'-0"	30'-6"
MAXIMUM AREA OF UNPROTECTED OPENINGS (BUILDING SPRINKLERED - 705.8.1)	25%	15%	NO LIMIT	NO LIMIT

SHAFTS CHAPTER 7
SHAFT CONSTRUCTION: 2 HR REQUIRED PER 713.4

CHUTES CHAPTER 7
CHUTE ENCLOSURE: 2 HR REQUIRED PER 713.4
HOPPER DOOR CONSTRUCTION: 1 1/2-HR AS REQUIRED PER TABLE 716.5.9.3

REFUSE COLLECTION ROOM: 1-HR FIRE BARRIER ENCLOSURE WITH 3/4-HR DOOR PER 708.13.3
DOOR OPERATION: SELF OR AUTOMATIC CLOSING UPON ACTIVATION OF SMOKE DETECTION PER 716.5.9.3

TERMINATION ROOM: SPRINKLERED PER TABLE 509 WITH 1 1/2-HR DOOR PER 713.13.4 & TABLE 716.5
SPRINKLERS: AUTOMATIC SPRINKLER SYSTEM PROVIDED PER 713.13.6

OPENINGS CHAPTER 7
WINDOWS: EMERGENCY WINDOWS PER SECTION 1029 ARE NOT REQUIRED PER 403.5.6
FIRE PROTECTION RATINGS FOR DOORS (TABLE 716.5)

FIRE SERVICE ACCESS ELEVATOR LOBBY DOORS	45 MINUTE "S" LABEL DOORS IN 1 HOUR SMOKE BARRIER
ELEVATOR SHAFT DOORS	90 MINUTE "S" LABEL DOORS IN 2-HR FIRE BARRIER
PRESSURIZED VESTIBULE DOORS FROM CORRIDOR	90 MINUTE "S" LABEL DOORS IN 2-HR FIRE BARRIER
PRESSURIZED VESTIBULE DOORS INTO STAIRWAY	90 MINUTE "S" LABEL DOORS IN 2-HR FIRE BARRIER
CORRIDOR DOORS	20 MINUTE "S" LABEL DOORS IN 1-HR FIRE PARTITION

ELEVATORS CHAPTERS 7, 10, & 30
LOBBY: ENCLOSED LOBBY NOT REQUIRED AT STREET LEVEL PER 713.14.1, EXCEPTION 1
 FIRE SERVICE ACCESS ELEVATOR LOBBY SIZE MIN DIMENSION OF 8'-0", AND 150 SF
ACCESSIBLE MEANS OF EGRESS ELEVATOR: ONE (1) ELEVATOR TO BE PROVIDED AS ACCESSIBLE MEANS OF EGRESS PER SECTION 1007.2.1

FLOOR LANDINGS ILLUMINATION: 5 fc MIN AT THRESHOLD (1124A.5)
ELEVATOR SIZE REQUIREMENT: ONE (1) ELEVATOR TO ACCOMMODATE 24-INCH BY 84-INCH AMBULANCE STRETCHER WITH 5-INCH MAXIMUM RADIUS CORNERS, 80-INCH BY 54-INCH CLEAR DISTANCE BETWEEN WALLS, AND MINIMUM DISTANCE FROM WALL TO RETURN PANEL OF 51 INCHES WITH A 42 INCH MINIMUM SIDE-SLIDE DOOR PER SECTION 3002.4.3a

ALL PROVIDED ELEVATORS TO MEET ACCESSIBILITY REQUIREMENTS OF CBC 11B-407 & 1124A
FIRE SERVICE ACCESS ELEVATORS: TWO REQUIRED PER 403.6.1
 CAPACITY: 3500# MIN PER 403.6.1
 FIRE ALARM TO INITIATE PHASE I EMERGENCY RECALL (3007.2)
 PROVIDE PHASE II EMERGENCY IN-CAR OPERATION (3003.2)
 MONITORED BY FIRE COMMAND CENTER (3007.8)
 STANDBY POWER TYPE 60/CLASS 2/LEVEL 1, REQUIRED FOR ELEVATOR EQUIPMENT, HOISTWAY LIGHTING, MACHINE ROOM VENTILATION AND COOLING EQUIPMENT, AND CONTROLLER COOLING EQUIPMENT (3007.9)

INTERIOR FINISHES CHAPTER 8

WALL/CEILING MAXIMUM FLAME SPREAD CLASSIFICATIONS AND RATINGS (TABLE 803.11)

EXIT ENCLOSURES:	CLASS B
CORRIDORS AND EXIT ACCESS ENCLOSURES:	CLASS C TYP
ROOMS AND ENCLOSED SPACES:	CLASS C

FLOOR FINISH (SECTION 804)

ALL AREAS:	COMPLY WITH ASTM STANDARD E 648, & SPECIFIC OPTICAL DENSITY SMOKE RATING NOT TO EXCEED 450 PER ASTM E662 (804.4.1)
EXITS, CORRIDORS, & ROOMS OR SPACES NOT SEPARATED FROM CORRIDORS:	MINIMUM CRITICAL RADIANT FLUX: CLASS II (804.4.2)

REQUIRED LIFE SAFETY SYSTEMS CHAPTER 9

	REQUIREMENT	TYPE/CLASS
AUTOMATIC SPRINKLER SYSTEM*	PER 903.2, 903.3.1.1, & SJFC 17.12.630	NFPA 13
STANDPIPE SYSTEM	PER 905.3.1, Exception 1	NFPA 14 / CLASS I
FIRE PUMPS	PER 913	NFPA 20
FIRE EXTINGUISHERS	PER 906.1 & CRC Title 19, Div 1, Chapter 3	2-A MIN RATED
FIRE ALARM SYSTEM**	PER 907.2	NFPA 72 AS AMENDED IN CHAPTER 35
AUDIBLE ALARMS	PER 907.5.2.1 & 907.5.2.2	
VISIBLE ALARMS	PER 907.5.2.3	
SMOKE DETECTION SYSTEM	PER 907.2.13 & 907.3	NFPA 72
FIRE DEPARTMENT COMMUNICATION SYSTEM	PER 907.2.13.2	NFPA 72

* SYSTEMS SERVING MORE THAN 20 HEADS SHALL BE SUPERVISED BY AN APPROVED CENTRAL, PROPRIETARY, OR REMOTE SERVICE
 QUICK RESPONSE OR RESIDENTIAL AUTOMATIC SPRINKLERS ARE REQUIRED IN DWELLING OR SLEEPING UNITS

** SYSTEM SHALL ACTIVATE A MEANS OF WARNING FOR THE HEARING IMPAIRED (1007.12)

SECONDARY WATER SUPPLY (903.3.5.2)
 SMOKE CONTROL SYSTEM (SECTION 909)
 FIRE DEPARTMENT CONNECTIONS (SECTION 912)
 EMERGENCY RESPONDER SAFETY FEATURES (SECTION 914)
 EMERGENCY RESPONDER RADIO COVERAGE (SECTION 915)
 STANDBY POWER SYSTEM (SECTION 2702)
 EMERGENCY POWER SYSTEM (SECTION 2702)
 PRESSURIZED EXIT ENCLOSURES (SECTIONS 909.20 AND 1022.10)
FIRE COMMAND CENTER (SECTION 911)
 ROOM SIZE MIN DIMENSION OF 10'-0", AND 200 SF
 FIREFIGHTER AIR REPLENISHMENT SYSTEM (2016 CALIFORNIA FIRE CODE - APPENDIX L)

F.A.R.S. - A PERMANENTLY INSTALLED ARRANGEMENT OF PIPING, VALVES, FITTINGS AND EQUIPMENT TO FACILITATE THE REPLENISHMENT OF BREATHING AIR IN SELF-CONTAINED BREATHING APPARATUS (SCBA) FOR FIRE FIGHTERS ENGAGED IN EMERGENCY OPERATIONS.

MEANS OF EGRESS CHAPTER 10

EGRESS WIDTH PER OCCUPANT SERVED (1005.3)
 STAIRWAYS: (1005.3.1 EXCEPTION 1) 2'0"OCC
 OTHER EGRESS COMPONENTS: (1005.3.2 EXCEPTION 1) .15'0"OCC

EXIT ACCESS (1006)
 COMMON PATH OF EGRESS TRAVEL PER TABLE 1006.2.1
 B & S OCCUPANCIES: 100'-0"
 A OCCUPANCIES: 75'-0"
 R-1 OCCUPANCIES: 75'-0"

ACCESSIBLE MEANS OF EGRESS (1009)
 48" STAIRWAY WIDTHS NOT REQUIRED (1009.3, EXCEPTION 2)
 AREAS OF REFUGE NOT REQUIRED (1009.3, EXCEPTION 5 & 1009.4, EXCEPTION 2)
 TWO WAY COMMUNICATION SYSTEM REQUIRED AT ELEVATOR LANDINGS (1009.8)

EXIT ACCESS TRAVEL DISTANCE (1017)
 A, R, S-1 OCCUPANCIES: 250'-0"
 B OCCUPANCY 300'-0"
 S-2 OCCUPANCY 400'-0"

MINIMUM CORRIDOR WIDTH (1018.2)

LOCATION	MIN WIDTH
ACCESS TO MECHANICAL, PLUMBING, OR ELECTRICAL SYSTEMS OR EQUIPMENT	24"
WHERE OCCUPANT CAPACITY IS LESS THAN 50	36"
WITHIN A DWELLING UNIT	36"
ALL OTHER LOCATIONS	44"

CORRIDOR DEAD END (1020.4):
 A OCCUPANCY: 20'-0"
 B, R-1, & S OCCUPANCIES: 50'-0" (1020.4, EXCEPTION 2)

LUMINOUS EGRESS PATH MARKING (1025)
 (NOT REQUIRED INSIDE GUEST ROOMS PER 1025.1, EXCEPTION 1)
 TYPICAL CONDITIONS: 1 fc MIN AT THE WALKING SURFACE TO PUBLIC WAY
 EMERGENCY POWER ILLUMINATION REQUIREMENTS PER 1025.2

INTERIOR ENVIRONMENT CHAPTER 12

LIGHTING OF OCCUPIED SPACES:
 NATURAL LIGHT: NET GLAZED AREA IS GREATER THAN 8% OF THE FLOOR AREA OF THE ROOM SERVED
 ARTIFICIAL LIGHT: 10fc AVERAGE PROVIDED AT 30-INCHES ABOVE THE FLOOR LEVEL

SOUND TRANSMISSION:
 WALLS: 50 STC (45 IF TESTED) MIN REQUIRED (1207.2)
 FLOOR/CEILINGS: 50 STC (45 IF TESTED) MIN REQUIRED (1207.2)
 INTERIOR NOISE LEVEL: 45 dBA DNL (GENERAL PLAN 2040 EC-1.1)

ROOFING REQUIREMENTS CHAPTERS 15 & 16

BASIC WIND SPEED 85 MPH (V3S)
 WIND EXPOSURE CATEGORY: B
 FIRE CLASSIFICATION: B

CLADDING REQUIREMENTS CHAPTERS 14 & 16

BASIC WIND SPEED 85 MPH (V3S)
 WIND EXPOSURE CATEGORY: B
 FIRE CLASSIFICATION: NON-COMBUSTIBLE



1645 NW HOYT
 PORTLAND OREGON 97209
 503 444 2200

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

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CODE ANALYSIS

SHEET NO.:

G100

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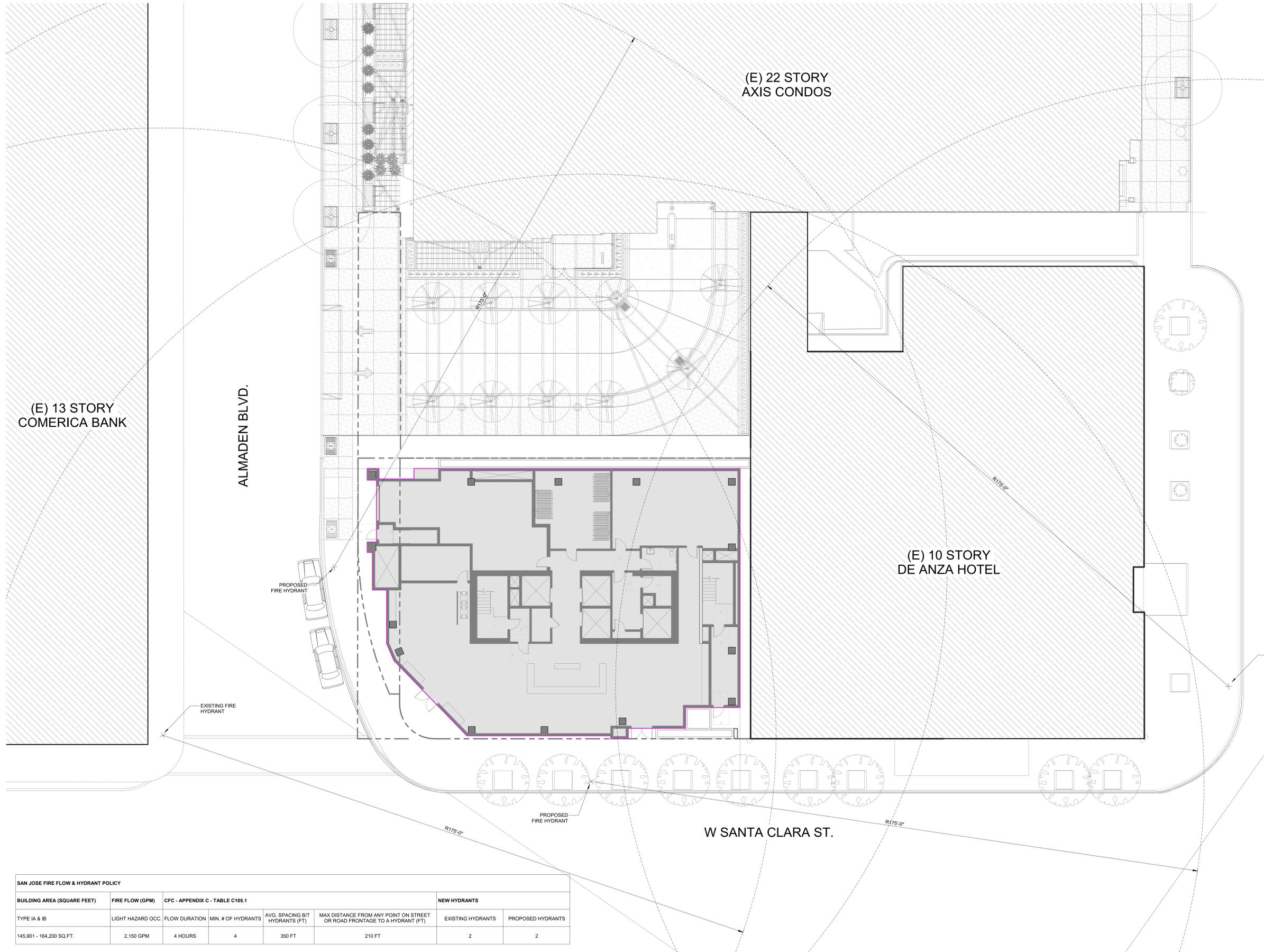
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SHEET TITLE:
FIRE LIFE SAFETY - SITE PLAN

SHEET NO.:
G150



SAN JOSE FIRE FLOW & HYDRANT POLICY

BUILDING AREA (SQUARE FEET)	FIRE FLOW (GPM)	CFC - APPENDIX C - TABLE C105.1				NEW HYDRANTS	
		FLOW DURATION	MIN. # OF HYDRANTS	AVG. SPACING (BT HYDRANTS (FT))	MAX DISTANCE FROM ANY POINT ON STREET OR ROAD FRONTAGE TO A HYDRANT (FT)	EXISTING HYDRANTS	PROPOSED HYDRANTS
TYPE IA & IB	LIGHT HAZARD OCC.	4 HOURS	4	350 FT	210 FT	2	2
145,901 - 164,200 SQ.FT.	2,150 GPM						



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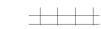
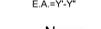
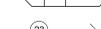
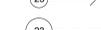
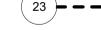
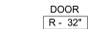
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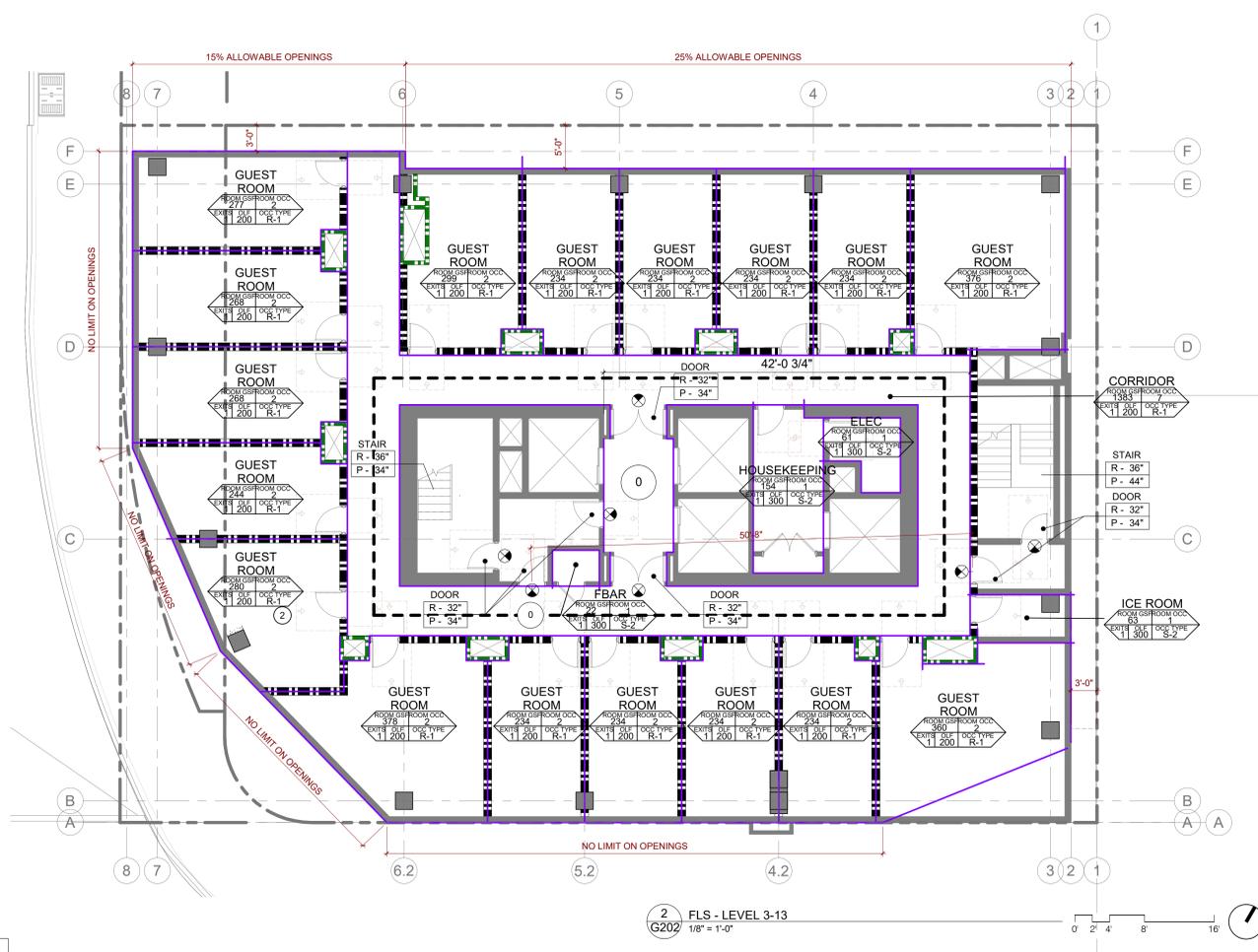
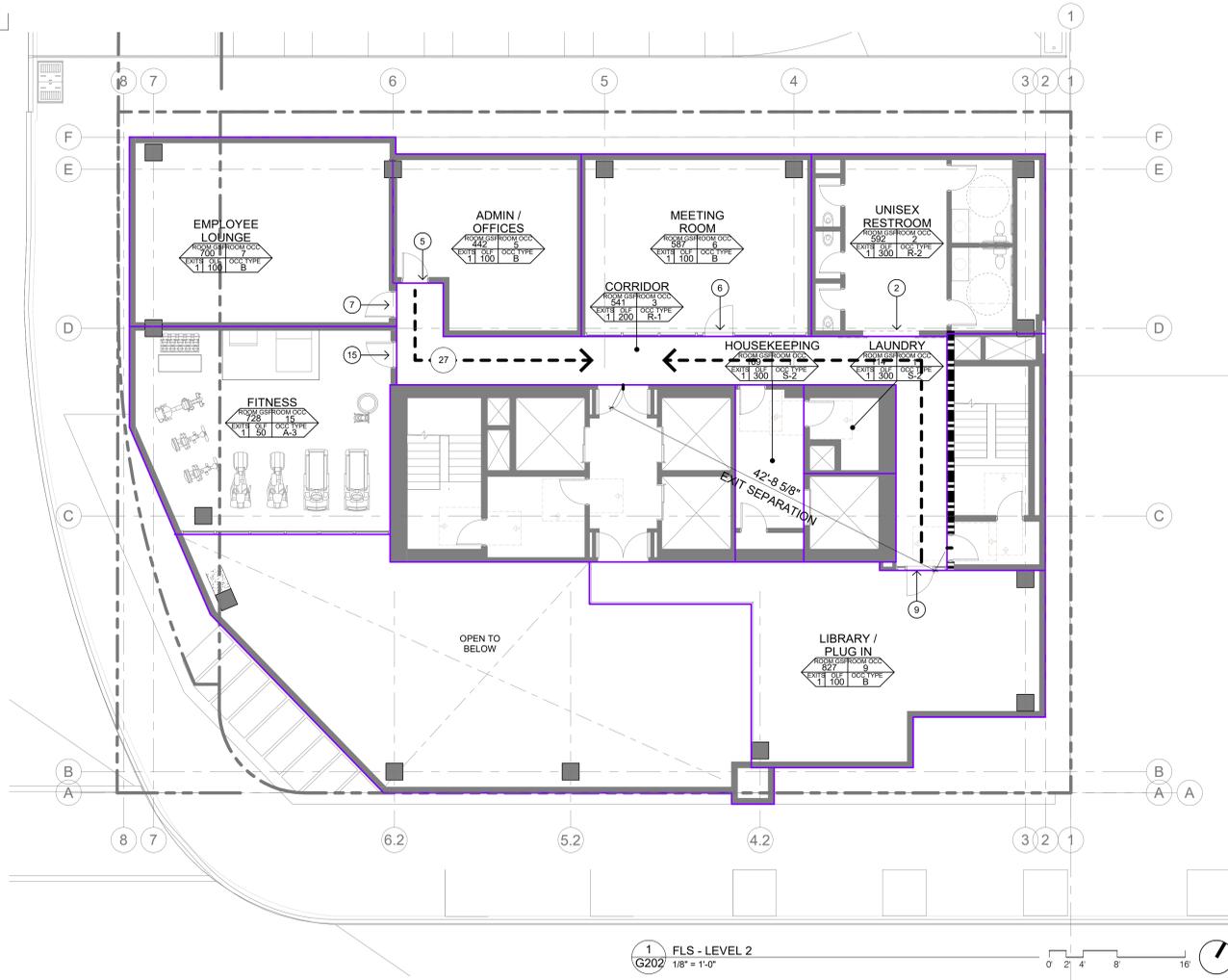
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FIRE LIFE SAFETY - LEVEL 2-13

SHEET NO.:
G202

FIRE LIFE SAFETY LEGEND

-  EXIT DISCHARGE
-  2 HOUR FIRE BARRIER ACTIVE SMOKE ZONE FOR STAIR & VESTIBULE PER CBC 909.20
-  ACTIVE SMOKE REMOVAL ZONE FOR GARAGE (CO REMOVAL SYSTEM)
-  EXIT FIXTURE, DOUBLE FACED, DIRECTION OF ARROW(S) AS SHOWN.
-  EXIT FIXTURE, SINGLE FACED, DIRECTION OF ARROW(S) AS SHOWN.
-  FIRE EXTINGUISHER CABINET, TYPE 2A-10BC EXTINGUISHER
-  FIRE EXTINGUISHER BRACKET-MTD, TYPE 2A-20BC
-  1-HR SEPARATION
-  2-HR SEPARATION
-  3-HR SEPARATION
-  1-HR SMOKE BARRIER, PER CBC 909.5 & SECTION 710
-  2-HR SMOKE BARRIER, PER CBC 909.5 & SECTION 710
-  PATH OF EGRESS
 C.P. = COMMON PATH DISTANCE
 E.A. = EXIT ACCESS DISTANCE
-  **Name**
 ROOM OR ROOM OCC.
 EXIT, D.F., OCC. TYPE
-  OCCUPANTS AT OCCUPANCY EXIT
-  TOTAL OCCUPANTS AT EXIT DISCHARGE
-  TOTAL OCCUPANTS IN AREA
-  DOOR
 R - 32"
 P - 34"
-  MINIMUM REQUIRED EGRESS WIDTH PROVIDED (CLEAR) EGRESS WIDTH

EGRESS WIDTH:
 CALCULATED BASED ON CBC SECTION 1005.3.1 STAIRWAYS, EXCEPTION 1
 0.2' PER OCCUPANT
 SECTION 1005.3.2 OTHER EGRESS COMPONENTS, EXCEPTION 1
 0.15' PER OCCUPANT



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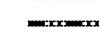
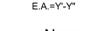
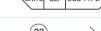
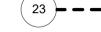
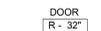
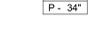
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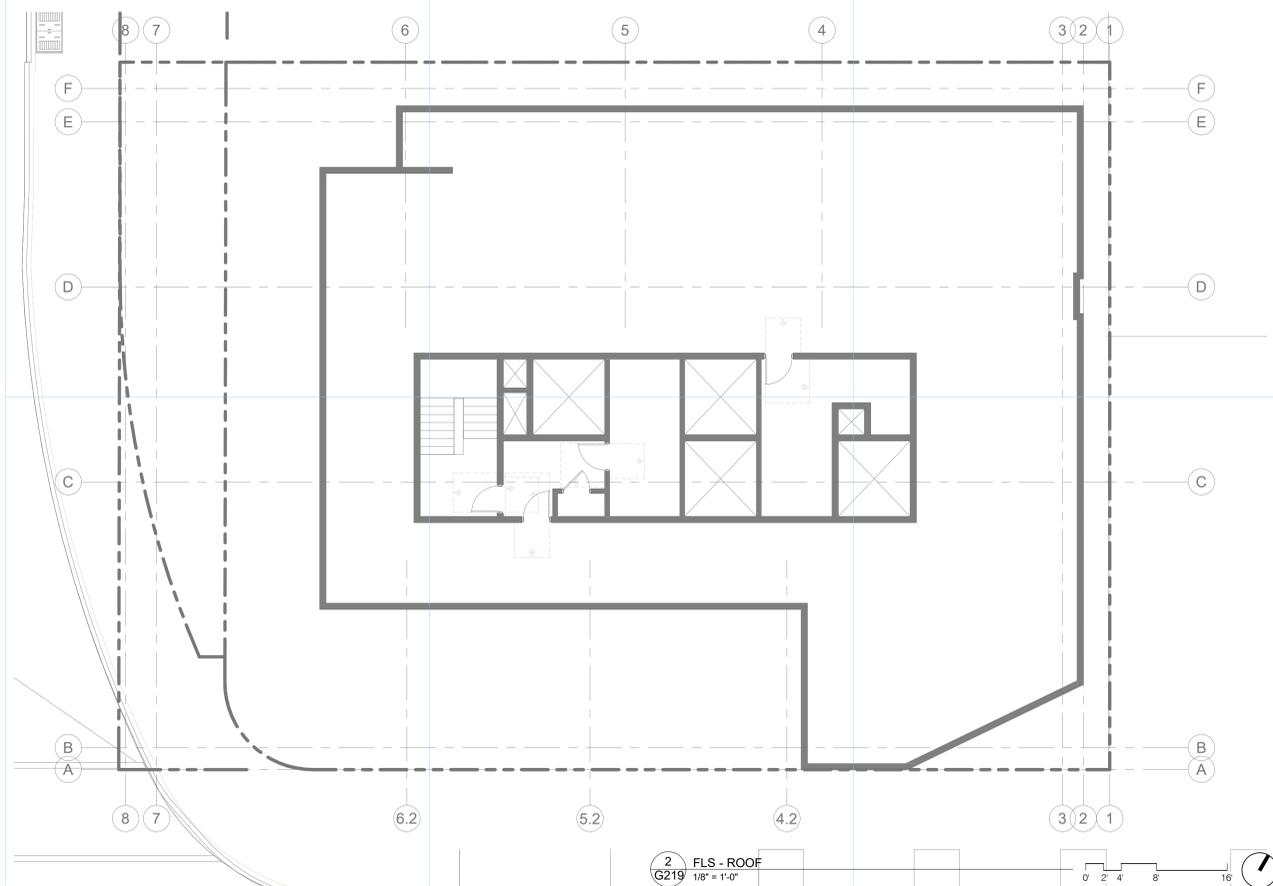
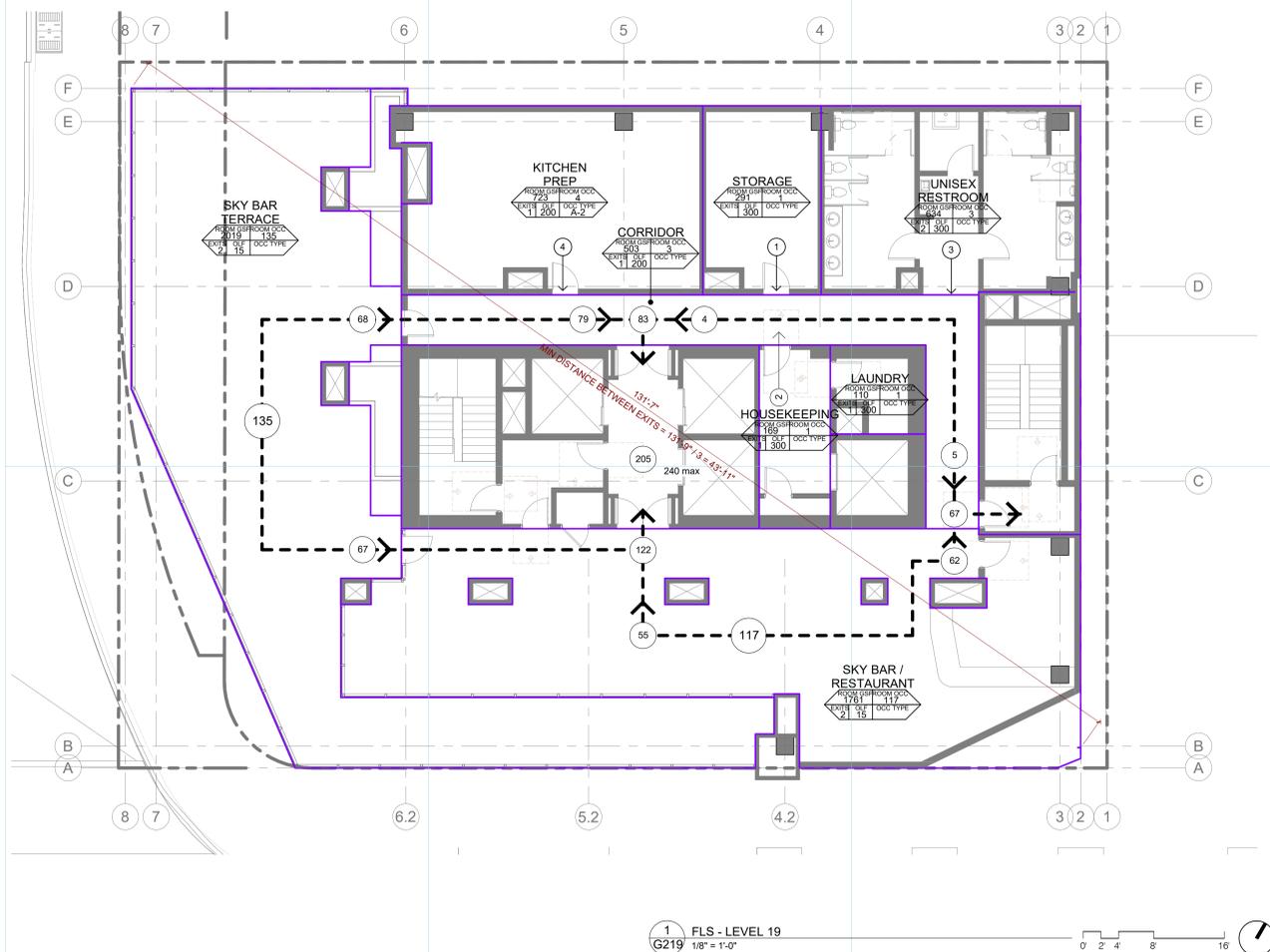
SHEET TITLE:
FIRE LIFE SAFETY - LEVEL 19-ROOF

SHEET NO.:

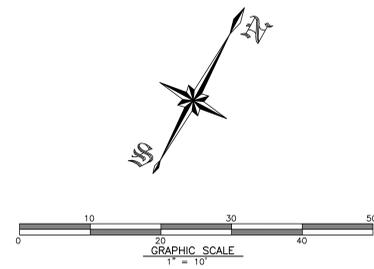
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FIRE LIFE SAFETY LEGEND

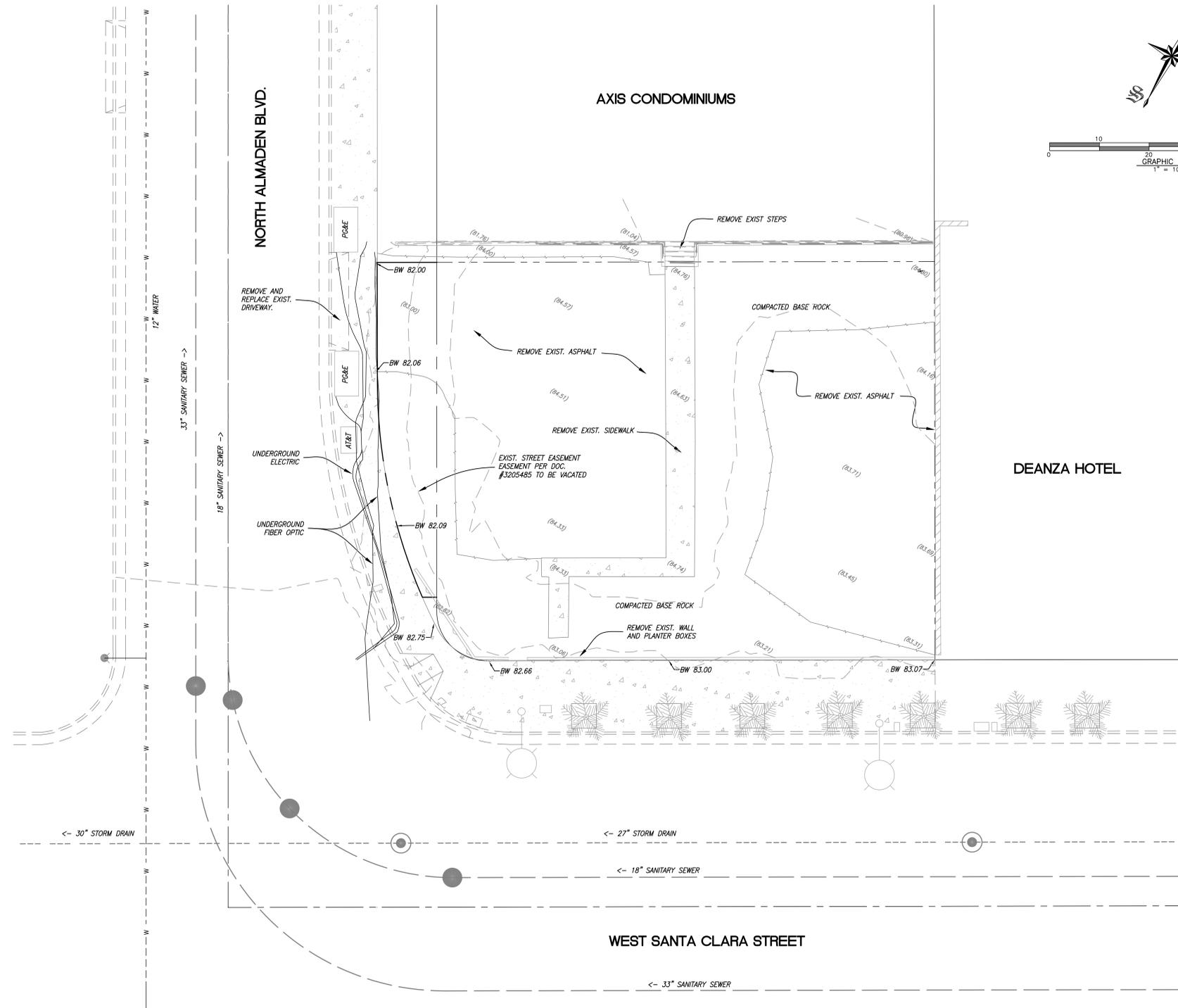
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-  ACTIVE SMOKE REMOVAL ZONE FOR GARAGE (CO REMOVAL SYSTEM)
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-  FIRE EXTINGUISHER CABINET, TYPE 2A-10BC EXTINGUISHER
-  FIRE EXTINGUISHER BRACKET-MTD, TYPE 2A-20BC
-  1-HR SEPARATION
-  2-HR SEPARATION
-  3-HR SEPARATION
-  1-HR SMOKE BARRIER, PER CBC 909.5 & SECTION 710
-  2-HR SMOKE BARRIER, PER CBC 909.5 & SECTION 710
-  PATH OF EGRESS
 C.P. = COMMON PATH DISTANCE
 E.A. = EXIT ACCESS DISTANCE
-  **Name**
 ROOM OR ROOM DOOR
 EXIT OR DOOR TYPE
-  OCCUPANTS AT OCCUPANCY EXIT
-  TOTAL OCCUPANTS AT EXIT DISCHARGE
-  TOTAL OCCUPANTS IN AREA
-  DOOR
 R - 32"
 P - 34"
-  MINIMUM REQUIRED EGRESS WIDTH PROVIDED (CLEAR) EGRESS WIDTH
-  EGRESS WIDTH:
 CALCULATED BASED ON CBC SECTION 1006.3.1 STAIRWAYS, EXCEPTION 1
 0.2" PER OCCUPANT
 SECTION 1006.3.2 OTHER EGRESS COMPONENTS, EXCEPTION 1
 0.15" PER OCCUPANT



H18-038 SITE DEVELOPMENT PERMIT



EXISTING	PROPOSED	DESCRIPTION
		PROPERTY LINE
		CENTERLINE
		CONTOUR
		CONCRETE CURB & GUTTER
		DRIVEWAY
		HANDICAP RAMP
		RETAINING WALL
		SANITARY SEWER
		SANITARY SEWER
		SANITARY MANHOLE
		SANITARY CLEANOUT
		STORM MANHOLE
		V-64 CHRISTY DRAIN OR EQUAL
		STANDARD HOODED INLET
		AREA DRAIN
		SIDEWALK
		POSITIVE RELEASE
		STREET LIGHT
		PALM TREE
		FINISHED FLOOR ELEVATION
		PAD ELEVATION
		TC 160.00
		FL 160.00
		SLOPE
		CB
		TW
		BW



BENCHMARK:
CSJ RM 126-S

THE LETTER 'F' IN THE WORD FOUNDRY ON TOP OF CATCH BASIN, SOUTHEAST RETURN OF POST STREET AND SAN PEDRO STREET. F.B. 1146, PG 2

ELEVATION 86.83 (NAVD88)

NOTE: TO CONVERT ELEVATIONS TO CSJ DATUM SUBTRACT 2.75 FEET

OWNER:
KT URBAN

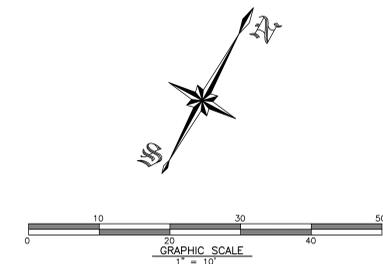
ADDRESS:
8 N ALMADEN BLVD
SAN JOSE, CA 95110

PROJECT NO.: 1847
DRAWN: PBS
DATE: 01/30/2019
SDP SUBMITTAL
REVISION: 6/10/19
DESCRIPTION: Respond to City Comments

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SDP SUBMITTAL

SHEET TITLE:
EXISTING CONDITIONS



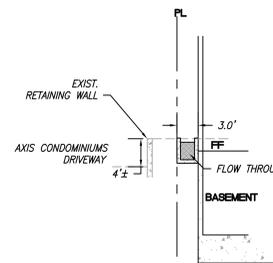
NOTES

- ELEVATIONS SHOWN ARE BASED ON THE NAVD88 DATUM. TO CONVERT TO THE NGVD29 DATUM SUBTRACT 2.75'
- TIEBACKS UNDER THE PUBLIC STREETS REQUIRE AN ENCROACHMENT PERMIT FROM THE CITY OF SAN JOSE
- TIEBACKS UNDER THE BUILDINGS TO THE NORTH AND EAST REQUIRE A SIGNED AGREEMENT FROM THE PROPERTY OWNER

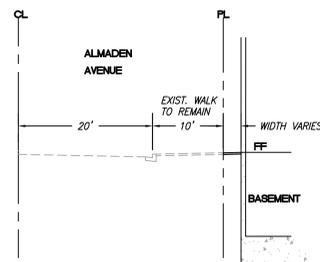
LEGEND		DESCRIPTION
EXISTING	PROPOSED	PROPERTY LINE
---	---	CENTERLINE
---	---	CONTOUR
---	---	CONCRETE CURB & GUTTER
---	---	DRIVEWAY
---	---	HANDICAP RAMP
---	---	RETAINING WALL
---	---	SANITARY SEWER
---	---	SANITARY MANHOLE
---	---	SANITARY CLEANOUT
---	---	STORM MANHOLE
---	---	V-64 CHRISTY DRAIN OR EQUAL
---	---	STANDARD HOODED INLET
---	---	AREA DRAIN
---	---	SIDEWALK
---	---	OVERLAND RELEASE
---	---	STREET LIGHT
---	---	PALM TREE
FF 161.00		FINISHED FLOOR ELEVATION
PAI 160.00		PAD ELEVATION
TC 160.00		TOP OF CURB ELEVATION
FL 160.00		FLOW LINE ELEVATION
S=0.004		SLOPE
GB		GRADE BREAK
TW		TOP OF WALL
BW		BOTTOM OF WALL

CONSTRUCTION NOTES

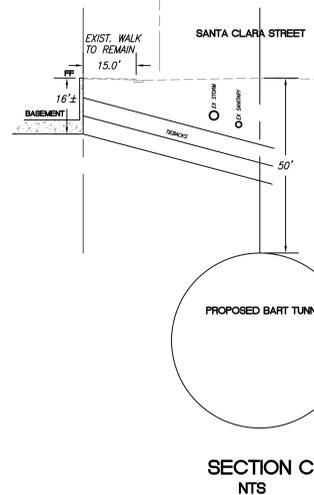
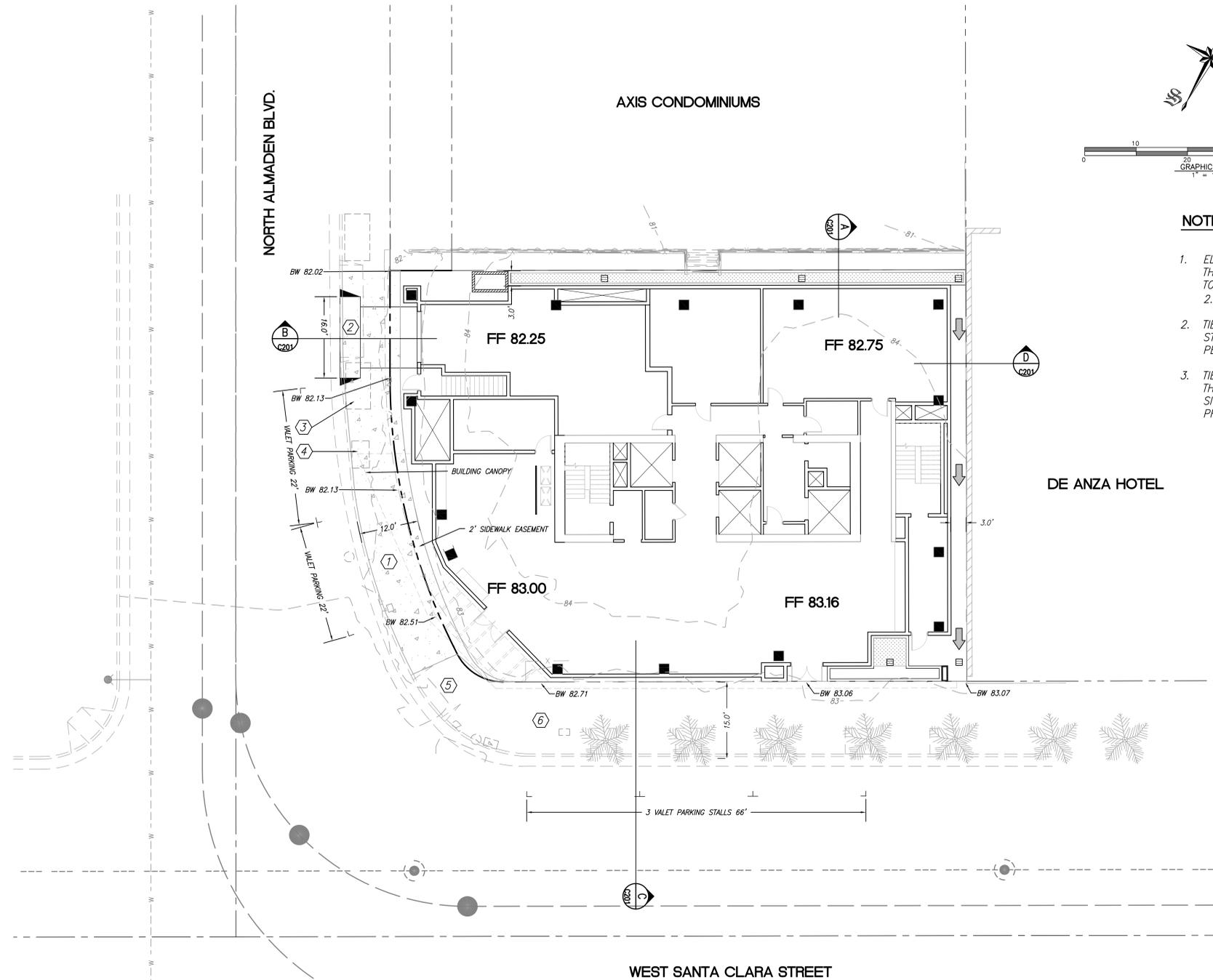
- CONSTRUCT 12' SIDEWALK PER CITY STANDARDS
- CONSTRUCT 16' WIDE COMMERCIAL DRIVEWAY
- RELOCATE UTILITY VAULT
- ADJUST ALL UTILITY VAULTS TO GRADE
- EXISTING RAMP TO REMAIN
- EXISTING SIDEWALK TO REMAIN



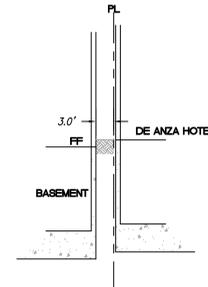
SECTION A
NTS



SECTION B
NTS



SECTION C
NTS



SECTION D
NTS

BENCHMARK:
CSJ RM 126-S

THE LETTER 'F' IN THE WORD FOUNDRY ON TOP OF CATCH BASIN, SOUTHEAST RETURN OF POST STREET AND SAN PEDRO STREET. F.B. 1146, PG 2

ELEVATION 86.83 (NAVD88)

NOTE: TO CONVERT ELEVATIONS TO CSJ DATUM SUBTRACT 2.75 FEET

ALMADEN CORNER HOTEL

OWNER:
KT URBAN

ADDRESS:
8 N ALMADEN BLVD
SAN JOSE, CA 95110

PROJECT NO.: 1847
 DRAWN: PBS
 DATE: 01/30/19
 SDP SUBMITTAL
 REVISION: 6/10/19
 DESCRIPTION: Respond to City Comments

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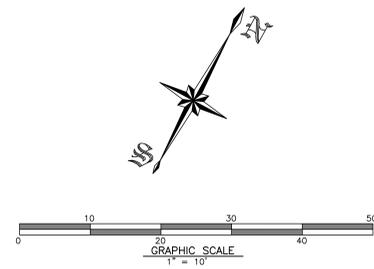
SHEET TITLE:
CONCEPTUAL GRADING PLAN

SHEET NO.:

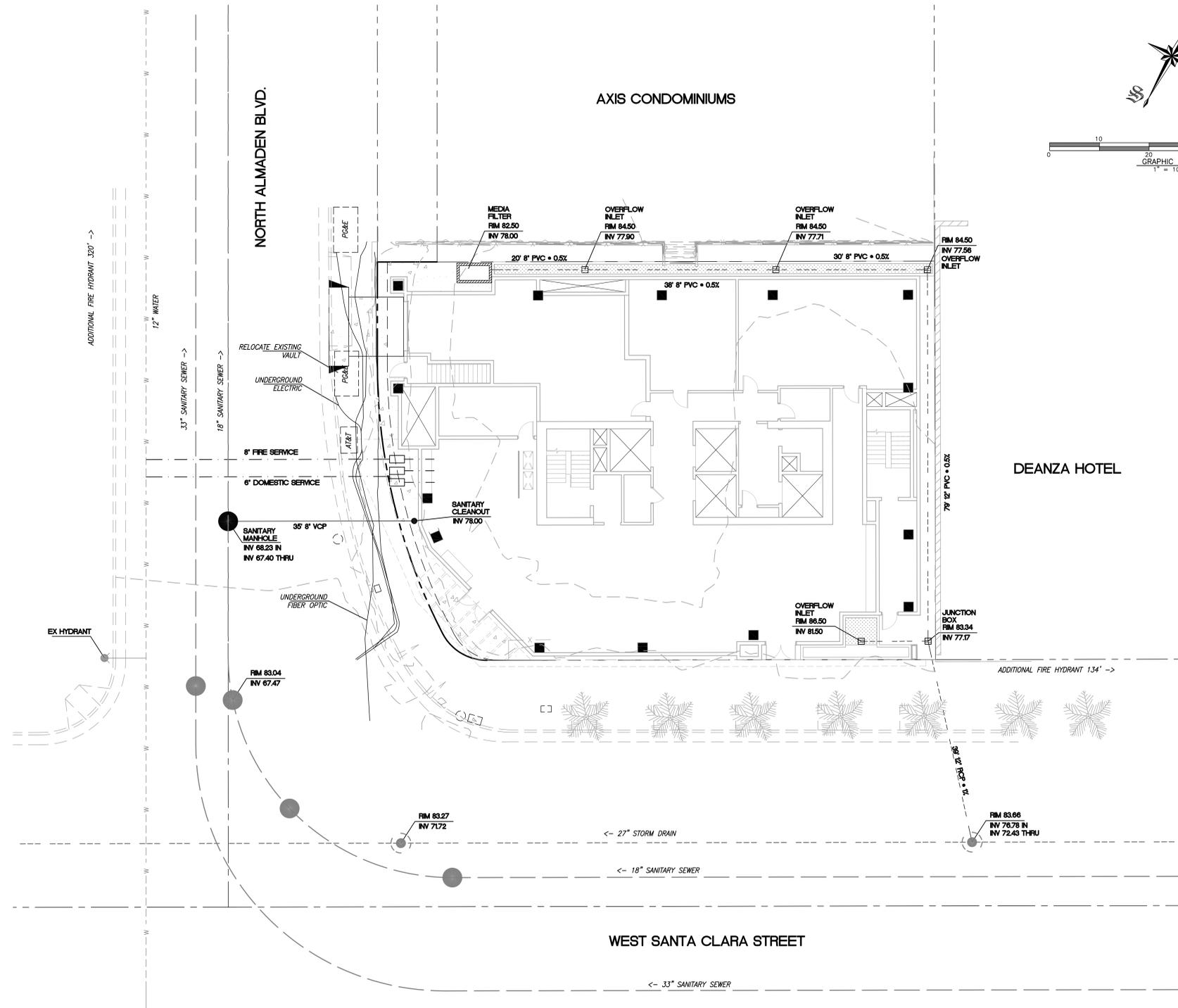
C201

Charles W. Davidson Co.

Consulting Civil Engineers
 A California Corporation
 255 W. Julian St
 San Jose California 95110
 (408) 295-9162



LEGEND		DESCRIPTION
EXISTING	PROPOSED	
---	---	PROPERTY LINE
---	---	CENTERLINE
---	---	CONTOUR
---	---	CONCRETE CURB & GUTTER
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---	---	PAD ELEVATION
---	---	TOP OF CURB ELEVATION
---	---	FLOW LINE ELEVATION
---	---	SLOPE
---	---	GRADE BREAK
---	---	TOP OF WALL
---	---	BOTTOM OF WALL



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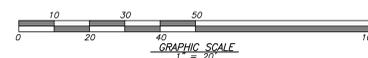
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SHEET TITLE:
CONCEPTUAL UTILITY PLAN

SHEET NO.:
C301

PRE-DEVELOPMENT

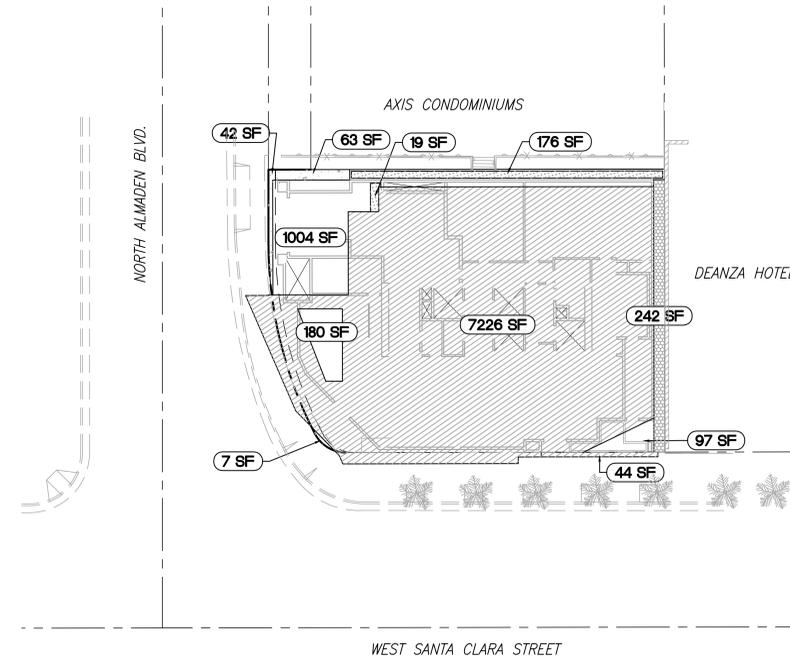


POST DEVELOPMENT



LEGEND

-  AC PARKING
-  COMPACTED BASEROCK
-  CONCRETE
-  ROOF
-  PERMEABLE PAVERS
-  TERRACE
-  LANDSCAPE



2. SURFACE DATA

2.a. Enter the Project Phase Number (1, 2, 3, etc. or N/A if Not Applicable): N/A

2.b. Total area of site: 0.21 acres

2.c. Total Existing Impervious Surfaces on site: 9,100 sq. ft.

2.d. Total area of site that will be disturbed: 0.21 acres

COMPARISON OF IMPERVIOUS AND PERVIOUS SURFACES AT PROJECT SITE	Existing Surface		Proposed Surface		RESET CALCULATIONS
	Area Disturbed sq. ft.	To Be Replaced sq. ft. ¹	New sq. ft. ²		
2.e. IMPERVIOUS SURFACES					
Roof Area		7,270			
Parking	5,022				
Sidewalks, Patios, Driveways, Etc.	3,726	1,344			
Public Streets	352	49			
Private Streets					
Impervious Surfaces Total	e.1. 9,100	e.2. 8,663	e.3. 0	e.4. 8,663	Total Proposed Impervious Surface (replaced + new)
<i>Online form auto-calculates</i>					
2.f. PERVIOUS SURFACES					
Landscaped Area			195		
Pervious Paving			242		
Green Roof and other Pervious Surfaces					Total Proposed Pervious Surface (replaced + new)
Pervious Surfaces Total	f.1. 0	f.2. 0	f.3. 437	f.4. 437	
<i>Online form auto-calculates</i>					
2.g. Percentage of Site's Impervious Area Replacement (e.2 + 2.c) X 100:	<i>Online form auto-calculates</i>			g. 95.20	%

¹ Proposed Replaced Impervious Surface: Replacement of an existing impervious surface with another impervious surface.
² Proposed New Impervious Surface: New impervious surface that will cover an existing pervious surface.

PROJECT SITE INFORMATION:

- SOILS TYPE: C
- GROUND WATER DEPTH: 15' TO 20'
- NAME OF RECEIVING BODY: GUADALUPE RIVER
- FLOOD ZONE: D
- FLOOD ELEVATION (IF APPLICABLE): N/A

SITE DESIGN MEASURES:

- REDUCE EXISTING IMPERVIOUS SURFACES.
- CREATE NEW PERVIOUS AREAS: LANDSCAPING
- DIRECT RUNOFF FROM ROOFS, SIDEWALKS, PATIOS TO LANDSCAPED AREAS.
- PARKING: NO PARKING PROVIDED

OPERATION AND MAINTENANCE INFORMATION:

- PROPERTY INFORMATION:
 - PROPERTY ADDRESS: NONE
 - APN: 259-35-055
 - PROPERTY OWNER: ALMADEN CORNER, LLC
21710 STEVENS CREEK BLVD, SUITE 200
CUPERTINO, CA 95014
- RESPONSIBLE PARTY FOR MAINTENANCE:
 - CONTACT: TBD
 - PHONE NUMBER OF CONTACT: _____
 - EMAIL: _____
 - ADDRESS: _____

SOURCE CONTROL MEASURES:

- CONNECT THE FOLLOWING FEATURES TO SANITARY SEWER:
 - COVERED TRASH/ RECYCLING ENCLOSURES.
 - COVERED LOADING DOCKS.
 - PUMPED GROUNDWATER.
- BENEFICIAL LANDSCAPING.
- USE OF WATER EFFICIENT IRRIGATION SYSTEMS.
- MAINTENANCE (PAVEMENT SWEEPING, CATCH BASIN CLEANING, GOOD HOUSEKEEPING).
- STORM DRAIN LABELING.

ALMADEN CORNER HOTEL

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 DRAWN: PBS
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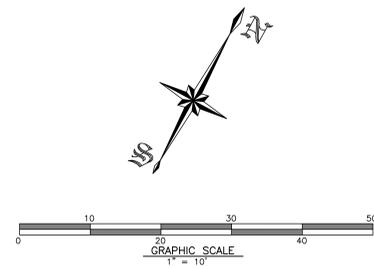
REVISION: DESCRIPTION:
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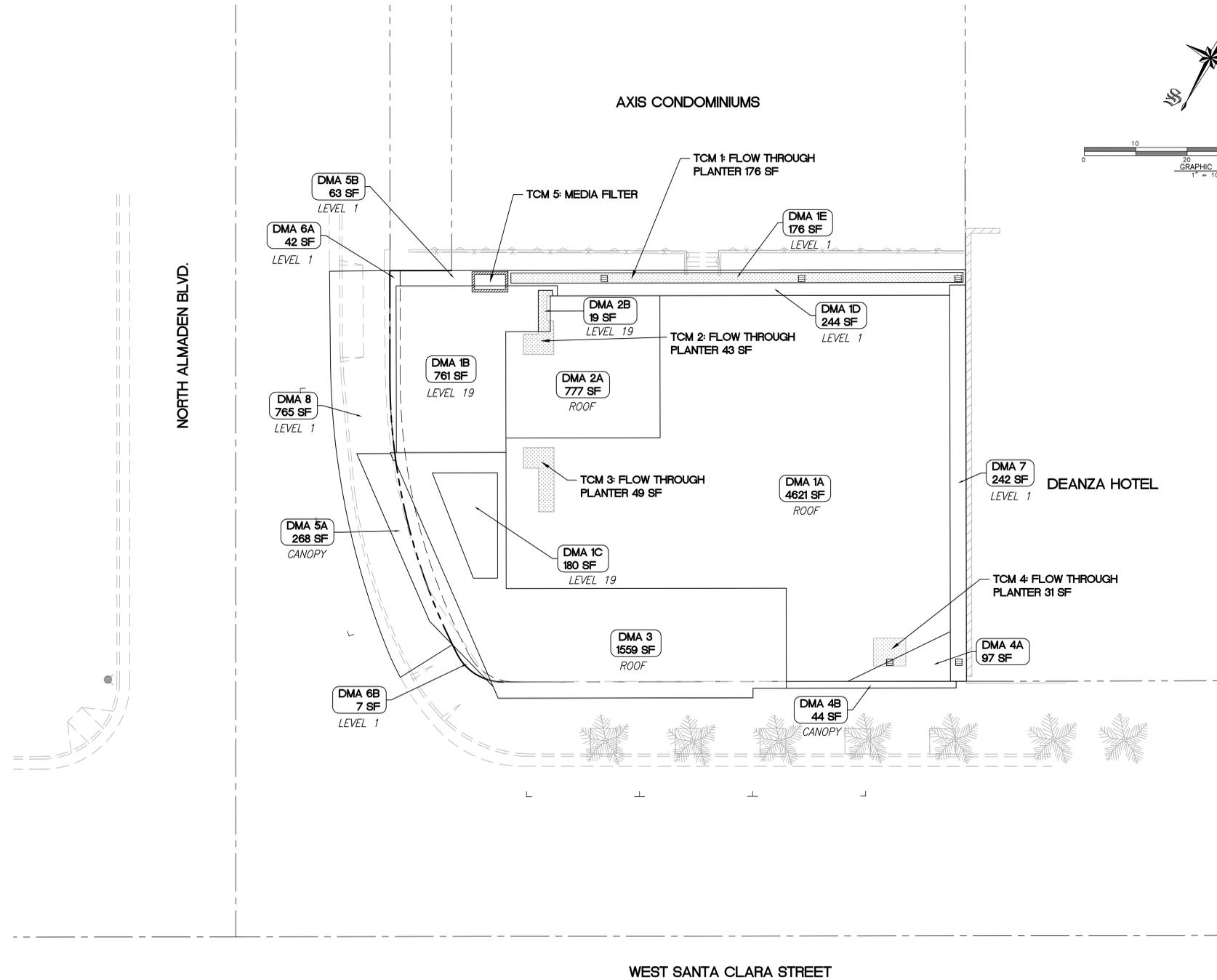
SDP SUBMITTAL

SHEET TITLE:
PERVIOUS - IMPERVIOUS COMPARISON PLAN

SHEET NO.:
C401



LEGEND		DESCRIPTION
EXISTING	PROPOSED	
---	---	PROPERTY LINE
---	---	CENTERLINE
---	---	CONTOUR
---	---	CONCRETE CURB & GUTTER
---	---	DRIVEWAY
---	---	HANDICAP RAMP
---	---	RETAINING WALL
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---	---	SANITARY SEWER MANHOLE
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---	---	V-64 CHRISTY DRAIN OR EQUAL
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---	---	AREA DRAIN
---	---	SIDEWALK
---	---	POSITIVE RELEASE
---	---	STREET LIGHT
---	---	PALM TREE
---	---	FINISHED FLOOR ELEVATION
---	---	PAD ELEVATION
---	---	TC 160.00
---	---	FL 160.00
---	---	SLOPE
---	---	GRADE BREAK
---	---	TOP OF WALL
---	---	BOTTOM OF WALL



TREATMENT CONTROL MEASURE SUMMARY TABLE

DMA #	TCM #	Location	Treatment Type	LID or Non-LID	Sizing Method	Drainage Area (s.f.)	Impervious Area (s.f.)	PerVIOUS Area (Permeable Pavement) (s.f.)	PerVIOUS Area (Other) (s.f.)	% Onsite Area Treated by LID or Non-LID TCM	Bioretention Area Required (s.f.)	Bioretention Area Provided (s.f.)	Overflow Riser Height (in)	Storage Depth Required (ft)	Storage Depth Provided (ft)	# of Cartridges Required	# of Cartridges Provided	Media Type	Cartridge Height (inches)	# of Credit Trees	Treatment Credit (s.f.)	Comments
1	1	Onsite	Flow-Through planter concrete lined* w/ underdrain	LID	3. Flow-Volume Combo	5,982	5,806		176	65.74%	170	176	6									FTP located outside of building on level 1
2	2	Onsite	Flow-Through planter concrete lined* w/ underdrain	LID	3. Flow-Volume Combo	796	777		19	8.75%	23	43	6									A portion of the planter is under the terrace roof
3	3	Onsite	Flow-Through planter concrete lined* w/ underdrain	LID	3. Flow-Volume Combo	1,559	1,559			17.13%	45	49	6									The planter is under the terrace roof
4	4	Onsite	Flow-Through planter concrete lined* w/ underdrain	LID	3. Flow-Volume Combo	141	141			1.55%	6	31	6									The planter is under the roof
5	5	Onsite	Proprietary Media Filter System (MFS)	Non-LID	2C. Flow: I = 0.2	331	331			3.64%						1	1	phosphoc	18			located on grade outside of building footprint
6	n/a	Onsite	Roadway Project***	Non-LID	N/A	49	49			0.54%												
7	n/a	Onsite	Self-treating areas	LID	N/A	242		242		2.66%				0.11	0.17							
8	n/a	Offsite	Maintenance	N/A	N/A	765	765															
Totals:						9,100	8,683	242	195	100.00%												

Footnotes:
 * "Lined" refers to an impermeable liner placed on the bottom of a Bioretention basin or a concrete Flow-Through Planter, such that no infiltration into native soil occurs.
 ** Sizing for Bioretention Area Required calculated using the 4% Method (Impervious Area x 0.04)
 *** Per Chapter 2.3 of the C3 Stormwater Handbook Roadway projects that add new sidewalk along an existing roadway are exempt from Provision C.3.c of the Municipal Stormwater Permit.

ALMADEN CORNER HOTEL

OWNER:
KT URBAN

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**8 N ALMADEN BLVD
 SAN JOSE, CA 95110**

PROJECT NO.: 1847
 DRAWN: PBS
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 SDP SUBMITTAL
 REVISION: DESCRIPTION:
 6/10/19 REVISED PER CITY COMMENTS
 8/16/19 REVISED PER CITY COMMENTS

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SDP SUBMITTAL

SHEET TITLE:
STORM WATER MANAGEMENT PLAN

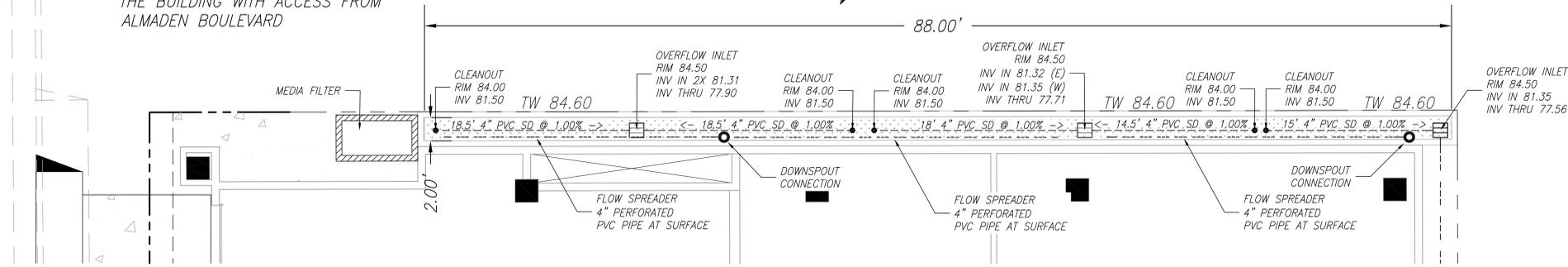
SHEET NO.:

C402

Charles W. Davidson Co.

Consulting Civil Engineers
 A California Corporation
 255 W. Julian St
 San Jose California 95110
 (408) 295-9162

NOTE: MEDIA FILTER TO BE LOCATED IN THE GROUND ADJACENT TO THE NORTH SIDE OF THE BUILDING WITH ACCESS FROM ALMADEN BOULEVARD



STANDARD STORMWATER CONTROL NOTES:

- STANDING WATER SHALL NOT REMAIN IN THE TREATMENT MEASURES FOR MORE THAN FIVE DAYS. TO PREVENT MOSQUITO GENERATION, SHOULD ANY MOSQUITO ISSUES ARISE, CONTACT THE SANTA CLARA VALLEY VECTOR CONTROL DISTRICT (DISTRICT). MOSQUITO LARVICIDES SHALL BE APPLIED ONLY WHEN ABSOLUTELY NECESSARY, AS INDICATED BY THE DISTRICT, AND THEN ONLY BY A LICENSED PROFESSIONAL OR CONTRACTOR. CONTACT INFORMATION FOR THE DISTRICT IS PROVIDED BELOW.
- DO NOT USE PESTICIDES OR OTHER CHEMICAL APPLICATIONS TO TREAT DISEASED PLANTS, CONTROL WEEDS OR REMOVE UNWANTED GROWTH. EMPLOY NON-CHEMICAL CONTROLS (BIOLOGICAL, PHYSICAL AND CULTURAL CONTROLS) TO TREAT A PEST PROBLEM. PRUNE PLANTS PROPERLY AND AT THE APPROPRIATE TIME OF YEAR. PROVIDE ADEQUATE IRRIGATION FOR LANDSCAPE PLANTS. DO NOT OVER WATER.

ALMADEN CORNER HOTEL

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BIORETENTION & FLOW-THROUGH PLANTER NOTES:

- SEE GRADING PLAN FOR BASIN FOOTPRINT AND DESIGN ELEVATIONS.
- PLACE 3 INCHES OF COMPOSTED, NON-FLOATABLE MULCH IN AREAS BETWEEN STORMWATER PLANTINGS.
- SEE LANDSCAPE PLAN FOR MULCH, PLANT MATERIALS AND IRRIGATION REQUIREMENTS
- CURB CUTS SHALL BE A MINIMUM 18" WIDE AND SPACED AT 10' O.C. INTERVALS AND SLOPED TO DIRECT STORMWATER TO DRAIN INTO THE BASIN. CURB CUTS SHALL ALSO NOT BE PLACED IN LINE WITH OVERFLOW CATCH BASIN. SEE GRADING PLAN FOR MORE DETAIL ON LOCATIONS OF CURB CUTS.
- A MINIMUM 0.2' DROP BETWEEN STORM WATER ENTRY POINT (I.E. CURB OPENING, FLUSH CURB, ETC.) AND ADJACENT LANDSCAPE FINISHED GRADE.
- DO NOT COMPACT NATIVE SOIL / SUBGRADE AT BOTTOM OF BASIN. LOOSEN SOIL TO 12" DEPTH.

BIOTREATMENT SOIL REQUIREMENTS

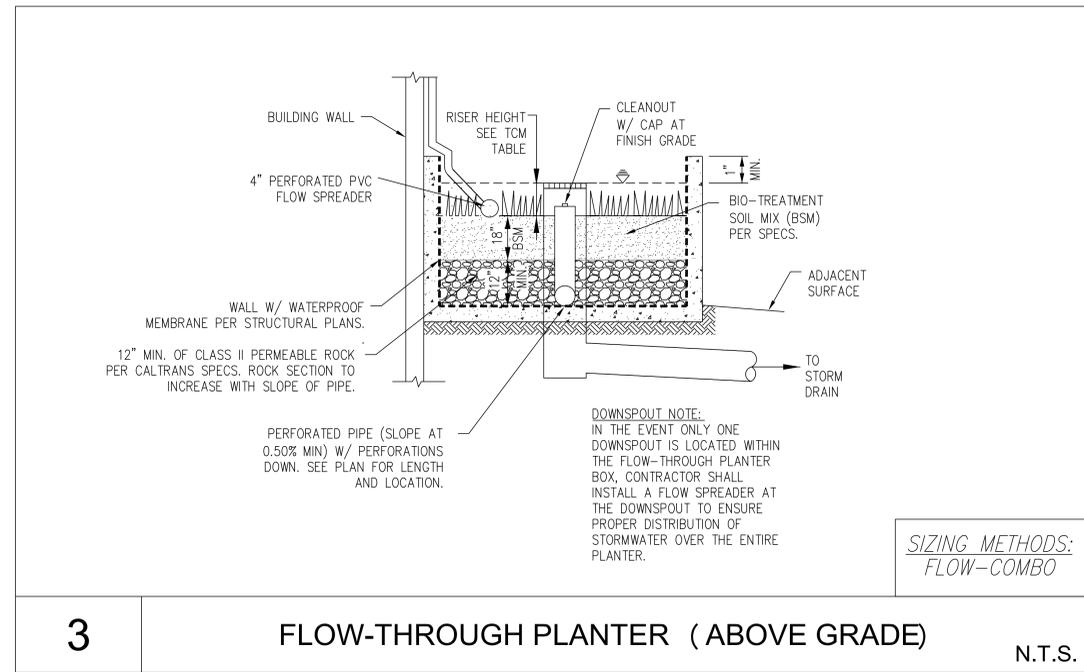
- BIORETENTION SOIL MIX SHALL MEET THE REQUIREMENTS AS OUTLINED IN APPENDIX C OF THE C.3 STORM WATER HANDBOOK AND SHALL BE A MIXTURE OF FINE SAND AND COMPOST MEASURED ON A VOLUME BASIS OF 60-70% SAND AND 30-40% COMPOST. CONTRACTOR TO REFER TO APPENDIX C FOR SAND AND COMPOST MATERIAL SPECIFICATIONS. CONTRACTOR MAY OBTAIN A COPY OF THE C3 HANDBOOK AT : [HTTP://WWW.SANJOSECA.GOV/INDEX.ASPX?NID=1761](http://www.sanjooseca.gov/index.aspx?nid=1761)
- PRIOR TO ORDERING THE BIOTREATMENT SOIL MIX OR DELIVERY TO THE PROJECT SITE, CONTRACTOR SHALL PROVIDE A BIOTREATMENT SOIL MIX SPECIFICATION CHECKLIST, COMPLETED BY THE SOIL MIX SUPPLIER AND CERTIFIED TESTING LAB.

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SDP SUBMITTAL

SHEET TITLE:
FLOW THROUGH PLANTER DETAILS

SHEET NO.:
C403



SIZING METHODS:
 FLOW-COMBO

3 FLOW-THROUGH PLANTER (ABOVE GRADE) N.T.S.

SIZING FOR VOLUME BASED TREATMENT	
DMA #	1
A=	5892 s.f.
Impervious Area =	5806 s.f.
% Imperviousness=	98.54%
MAPsite =	15
MAPpage =	13.9
Correction Factor=	1.0791
Clay (D):	X
Sandy Clay (D):	
Clay Loam (D):	
Silt Loam/Loam (B):	
Not Applicable (100% Impervious):	
Are the soils outside the building footprint not graded/compacted?	N Yes/No
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S=	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.57247522 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.59562118 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.57247522 inches (Corrected Slope for the site)
Adjusted UBS =	Correction Factor (Step 2) x UBSX% (Step 5)
Adjusted UBS =	0.617779 inches
Design Volume =	Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch
Design Volume =	303.33 ft ³
COMBO FLOW & VOLUME BIORETENTION CALCULATION	
Total Drainage Area =	5,892 sq. ft
Impervious Area =	5,806 sq. ft
Pervious Area =	86 sq. ft
Equivalent Impervious Area =	9 sq. ft
Total Equivalent Impervious =	5,815 sq. ft
Rainfall intensity =	0.2 in/hr
Duration =	Adjusted UBS (Step 6) / Rainfall Intensity
Duration =	3.0888951 hrs
Estimate the Surface Area =	176 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	226.51897 cu. ft
Volume in Ponding Area =	76.810524 cu. ft
Depth of Ponding =	0.4364234 ft
Depth of Ponding =	5.2 inches (Round up)

TABLE 1		ROUTINE MAINTENANCE
NO.	MAINTENANCE TASK	FREQUENCY OF TASK
1	INSPECT THE PLANTER SURFACE AREA, INLETS AND OUTLETS FOR OBSTRUCTIONS AND TRASH. CLEAR ANY OBSTRUCTIONS AND REMOVE TRASH.	QUARTERLY
2	INSPECT PLANTER FOR STANDING WATER. IF STANDING WATER DOES NOT DRAIN WITHIN 2-3 DAYS, THE SURFACE BIOTREATMENT SOIL SHOULD BE TILLED OR REPLACED WITH THE APPROVED SOIL MIX AND REPLANTED. USE THE CLEANOUT RISER TO CLEAR ANY UNDERDRAINS OF OBSTRUCTIONS OR CLOGGING MATERIAL.	QUARTERLY
3	CHECK FOR ERODED OR SETTLED BIOTREATMENT SOIL MEDIA. LEVEL SOIL WITH RAKE AND REMOVE REPLANT VEGETATION AS NECESSARY.	QUARTERLY
4	MAINTAIN THE VEGETATION AND IRRIGATION SYSTEM. PRUNE AND WEED TO KEEP FLOW-THROUGH PLANTER NEAT AND ORDERLY IN APPEARANCE.	QUARTERLY
5	EVALUATE HEALTH AND DENSITY OF VEGETATION. REMOVE AND REPLACE ALL DEAD AND DISEASED VEGETATION. REMOVE EXCESSIVE GROWTH OF PLANTS THAT ARE TOO CLOSE TOGETHER.	ANNUALLY, BEFORE THE RAINY SEASON BEGINS
6	USE COMPOST AND OTHER NATURAL SOIL AMENDMENTS AND FERTILIZERS INSTEAD OF SYNTHETIC FERTILIZERS, ESPECIALLY IF THE SYSTEM USES AN UNDERDRAIN.	ANNUALLY, BEFORE THE RAINY SEASON BEGINS
7	INSPECT THE OVERFLOW PIPE TO MAKE SURE THAT IT CAN SAFELY CONVEY EXCESS FLOWS TO A STORM DRAIN. REPAIR OR REPLACE ANY DAMAGED OR DISCONNECTED PIPING. USE THE CLEANOUT RISER TO CLEAR UNDERDRAINS OF OBSTRUCTIONS OR CLOGGING MATERIAL.	ANNUALLY, BEFORE THE RAINY SEASON BEGINS
8	INSPECT THE ENERGY DISSIPATOR AT THE INLET TO ENSURE IT IS FUNCTIONING ADEQUATELY, AND THAT THERE IS NO SCOUR OF THE SURFACE MULCH. REMOVE ANY ACCUMULATION OF SEDIMENT.	ANNUALLY, BEFORE THE RAINY SEASON BEGINS
9	INSPECT AND, IF NEEDED, REPLACE WOOD MULCH. IT IS RECOMMENDED THAT 2" TO 3" OF COMPOSTED ARBOR MULCH BE APPLIED ONCE A YEAR.	ANNUALLY, BEFORE THE RAINY SEASON BEGINS
10	INSPECT SYSTEM FOR EROSION OF BIOTREATMENT SOIL MEDIA, LOSS OF MULCH, STANDING WATER, CLOGGED OVERFLOWS, WEEDS, TRASH AND DEAD PLANTS. IF USING ROCK MULCH, CHECK FOR 3" OF COVERAGE.	ANNUALLY AT THE END OF THE RAINY SEASON AND/OR AFTER LARGE STORM EVENTS
11	INSPECT SYSTEM FOR STRUCTURAL INTEGRITY OF WALLS, FLOW SPREADERS, ENERGY DISSIPATORS, CURB CUTS, OUTLETS AND FLOW SPLITTERS.	ANNUALLY AT THE END OF THE RAINY SEASON AND/OR AFTER LARGE STORM EVENTS.

Charles W. Davidson Co.

Consulting Civil Engineers
 A California Corporation
 255 W. Julian St
 San Jose California 95110
 (408) 295-9162

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110**

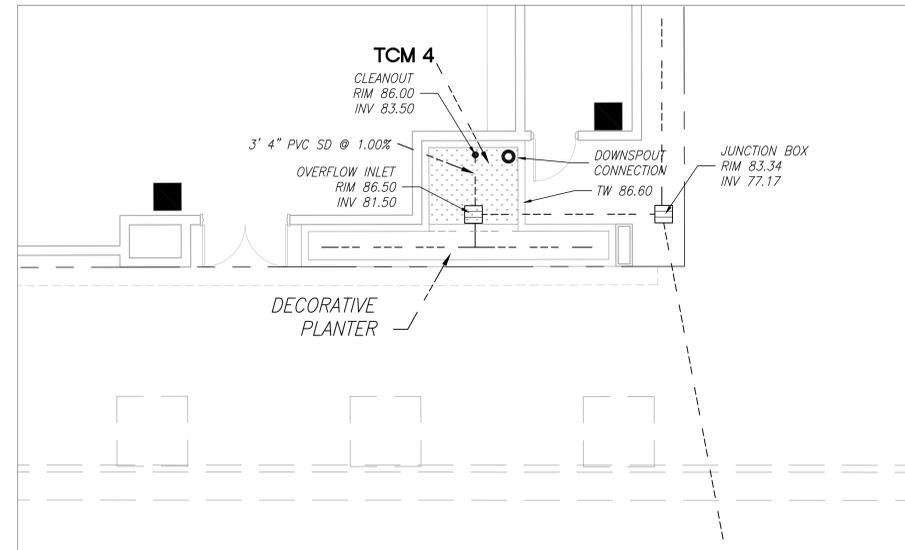
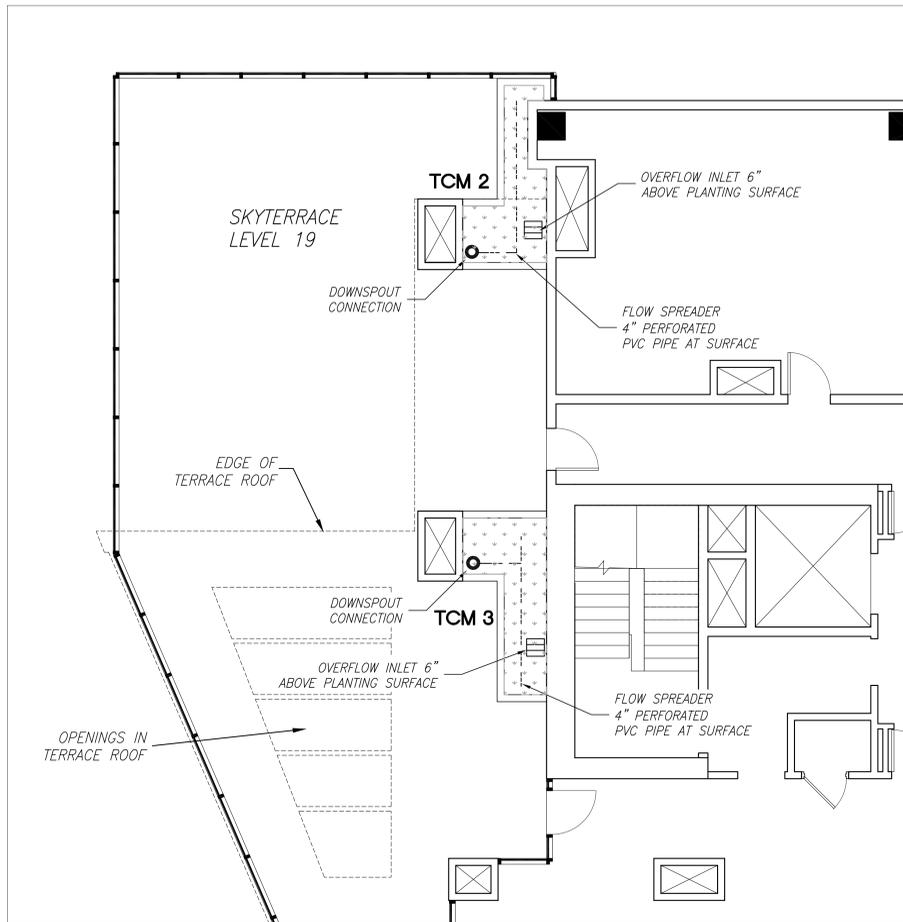
PROJECT NO.: 1847
 DRAWN: PBS
 DATE: 01/30/2019
 SDP SUBMITTAL
 REVISION: DESCRIPTION:
 6/10/19 REVISED PER CITY COMMENTS

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SHEET TITLE:
**FLOW THROUGH PLANTER
 DETAILS**

SHEET NO.:

C404

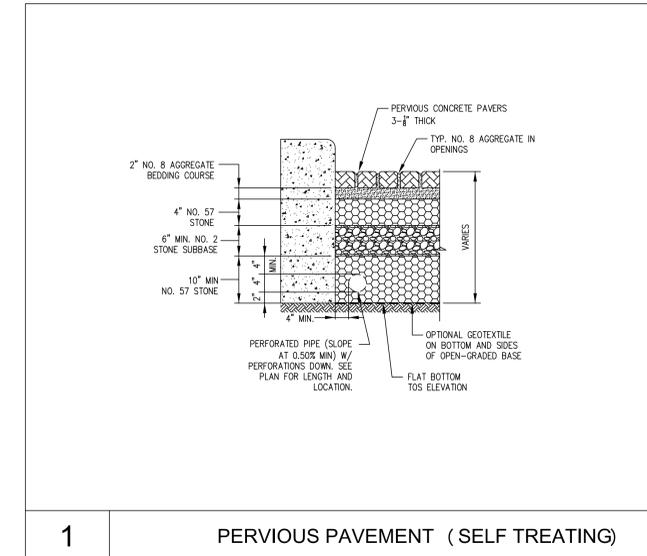
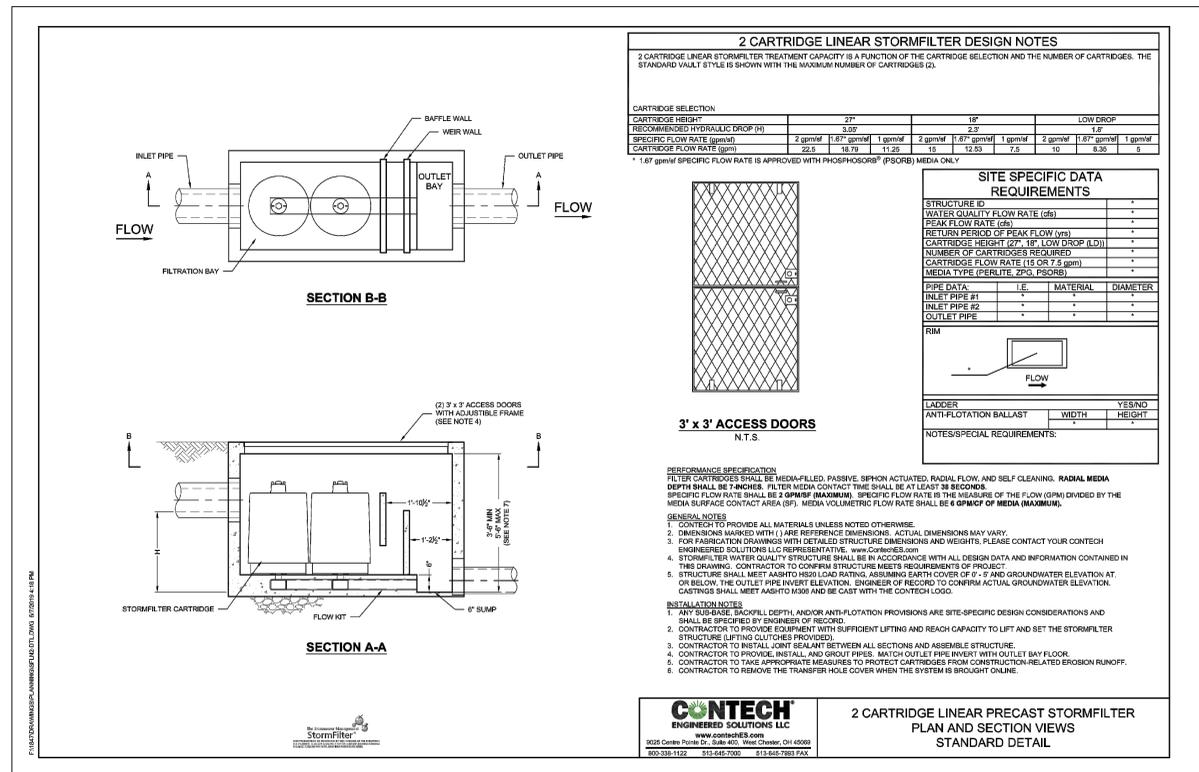


SIZING FOR VOLUME BASED TREATMENT	
DMA #	2
A =	796 s.f.
Impervious Area =	777 s.f.
% Imperviousness =	97.61%
MAPsite =	15
Correction Factor =	1.0791
MAPgage =	13.9
Clay (D):	X
Sandy Clay (D):	
Clay Loam (D):	
Silt Loam/Loam (B):	
Not Applicable (100% Impervious):	
Are the soils outside the building footprint not graded/compacted?	N Yes/No
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S =	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.5696005 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.5928392 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.5696005 inches (Corrected Slope for the site)
Adjusted UBS =	0.6146768 inches
Design Volume = Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch	
Design Volume =	40.77 ft ³
COMBO FLOW & VOLUME BIORETENTION CALCULATION	
Total Drainage Area =	796 sq. ft
Impervious Area =	777 sq. ft
Pervious Area =	19 sq. ft
Equivalent Impervious Area =	2 sq. ft
Total Equivalent Impervious =	779 sq. ft
Rainfall intensity =	0.2 in/hr
Duration = Adjusted UBS (Step 6) / Rainfall Intensity	
Duration =	3.073384 hrs
Estimate the Surface Area =	43 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	55.064797 cu. ft
Volume in Ponding Area =	-14.291236 cu. ft
Depth of Ponding =	-0.3323543 ft
Depth of Ponding = #NUM!	inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

SIZING FOR VOLUME BASED TREATMENT	
DMA #	3
A =	1559 s.f.
Impervious Area =	1559 s.f.
% Imperviousness =	100.00%
MAPsite =	15
Correction Factor =	1.0791
MAPgage =	13.9
Clay (D):	X
Sandy Clay (D):	
Clay Loam (D):	
Silt Loam/Loam (B):	
Not Applicable (100% Impervious):	
Are the soils outside the building footprint not graded/compacted?	N Yes/No
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S =	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.577 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.6 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.577 inches (Corrected Slope for the site)
Adjusted UBS =	0.6226619 inches
Design Volume = Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch	
Design Volume =	80.89 ft ³
COMBO FLOW & VOLUME BIORETENTION CALCULATION	
Total Drainage Area =	1559 sq. ft
Impervious Area =	1559 sq. ft
Pervious Area =	0 sq. ft
Equivalent Impervious Area =	0 sq. ft
Total Equivalent Impervious =	1,559 sq. ft
Rainfall intensity =	0.2 in/hr
Duration = Adjusted UBS (Step 6) / Rainfall Intensity	
Duration =	3.1133094 hrs
Estimate the Surface Area =	49 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	63.563399 cu. ft
Volume in Ponding Area =	17.330755 cu. ft
Depth of Ponding =	0.3536889 ft
Depth of Ponding =	4.2 inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

SIZING FOR VOLUME BASED TREATMENT	
DMA #	4
A =	141 s.f.
Impervious Area =	141 s.f.
% Imperviousness =	100.00%
MAPsite =	15
Correction Factor =	1.0791
MAPgage =	13.9
Clay (D):	X
Sandy Clay (D):	
Clay Loam (D):	
Silt Loam/Loam (B):	
Not Applicable (100% Impervious):	
Are the soils outside the building footprint not graded/compacted?	N Yes/No
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S =	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.577 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.6 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.577 inches (Corrected Slope for the site)
Adjusted UBS =	0.6226619 inches
Design Volume = Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch	
Design Volume =	7.32 ft ³
COMBO FLOW & VOLUME BIORETENTION CALCULATION	
Total Drainage Area =	141 sq. ft
Impervious Area =	141 sq. ft
Pervious Area =	0 sq. ft
Equivalent Impervious Area =	0 sq. ft
Total Equivalent Impervious =	141 sq. ft
Rainfall intensity =	0.2 in/hr
Duration = Adjusted UBS (Step 6) / Rainfall Intensity	
Duration =	3.1133094 hrs
Estimate the Surface Area =	31 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	40.213579 cu. ft
Volume in Ponding Area =	-32.897302 cu. ft
Depth of Ponding =	-1.0612033 ft
Depth of Ponding = #NUM!	inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

SDP SUBMITTAL



PERVIOUS PAVER REQUIREMENTS

CONTRACTOR OR PERMITEE SHALL:

- PROVIDE CERTIFICATION FROM THE PAVER MANUFACTURER THAT THE PAVERS MEET THE REQUIREMENTS OF THE C3 STORMWATER HANDBOOK FOR PERVIOUS PAVERS. THIS INCLUDES, BUT IS NOT LIMITED TO, HAVING A MINIMUM SURFACE INFILTRATION RATE OF 100% WHEN TESTED IN ACCORDANCE WITH ASTM C1701.
- ONLY CONTRACTORS HOLDING CERTIFICATION OF COMPLETION IN THE INTERLOCKING CONCRETE PAVEMENT INSTITUTES PICP INSTALLER TECHNICIAN COURSE SHALL BE USED TO INSTALL THE PAVERS AND AT LEAST ONE FOREMAN WITH THIS CERTIFICATION MUST BE ON THE JOBSITE AT ALL TIMES DURING CONCRETE PAVEMENT INSTALLATION.
- PROTECT THE ENCASED AREA FOR PERVIOUS PAVERS FROM EXCESSIVE COMPACTION DUE TO CONSTRUCTION TRAFFIC AND PROTECT THE FINISHED PAVEMENT FROM CONSTRUCTION TRAFFIC.

TABLE 1

NO.	ACTIVITIES FOR PERVIOUS PAVEMENT	
	MAINTENANCE TASK	FREQUENCY OF TASK
1	CHECK FOR SEDIMENT AND DEBRIS ACCUMULATION. PREVENT SOIL FROM WASHING OR BLOWING ONTO THE PAVEMENT. DO NOT STORE SAND, SOIL, MULCH OR OTHER LANDSCAPING MATERIALS ON PERVIOUS PAVEMENT SURFACES.	TWO TO FOUR TIMES ANNUALLY
2	CONDUCT PREVENTATIVE SURFACE CLEANING, USING COMMERCIALY AVAILABLE REGENERATIVE AIR OR VACUUM SWEEPERS, TO REMOVE SEDIMENT AND DEBRIS.	TWO TO FOUR TIMES ANNUALLY
3	INSPECT FOR ANY SIGNS OF PAVEMENT FAILURE. REPAIR ANY SURFACE DEFORMATIONS OR BROKEN PAVERS. REPLACE MISSING JOINT FILLER IN PICP.	TWO TO FOUR TIMES ANNUALLY
4	CHECK FOR STANDING WATER ON THE PAVEMENT SURFACE WITHIN 30 MINUTES AFTER A STORM EVENT.	TWO TO FOUR TIMES ANNUALLY
5	INSPECT UNDERDRAIN OUTLETS AND CLEANOUTS, PREFERABLY BEFORE THE WET SEASON. REMOVE TRASH/DEBRIS.	TWO TO FOUR TIMES ANNUALLY
6	REMOVE SEDIMENT AND DEBRIS ACCUMULATION ON PERVIOUS PAVEMENT.	TWO TO FOUR TIMES ANNUALLY
7	REMOVE WEEDS. MOW VEGETATION IN GRID PAVEMENTS (SUCH AS TURF BLOCK) AS NEEDED.	AS NEEDED
8	PERFORM RESTORATIVE SURFACE CLEANING WITH A VACUUM SWEEPER, AND/OR RECONSTRUCTION OF PART OF THE PERVIOUS SURFACE TO RESTORE SURFACE PERMEABILITY AS NEEDED. REPLENISH AGGREGATE IN PICP JOINTS OR GRIDS AS NEEDED AFTER RESTORATIVE SURFACE CLEANING.	AS NEEDED
9	POWER WASHING WITH SIMULTANEOUS VACUUMING ALSO CAN BE USED TO RESTORE SURFACE INFILTRATION TO HIGHLY CLOGGED AREAS OF PERVIOUS CONCRETE, POROUS ASPHALT OR PICP, BUT IS NOT RECOMMENDED FOR GRID PAVEMENTS.	AS NEEDED
10	INSPECT PERVIOUS PAVING AREA USING THE ATTACHED INSPECTION CHECKLIST.	QUARTERLY OR AS NEEDED

MEDIA FILTER SIZING

DMA # 4A-4D
 A = 328 s.f. A = 0.00753 acre

C Value	Area* (s.f.)	Weighted C Value	Rainfall Intensity (i)
0.9	328	0.900	0.2
0.8	0		
0.7	0		
0.1	0		

* Input Values by hand or use Table at the bottom of the spreadsheet.

Q = C x i x A
 Q = 0.0013554 cfs

Manufacturer: CONTECH
 Cartridge Height: 18 in.
 Cartridge Media (if applicable): Phosphosorb
 G.U.L.D. Cartridge Treatment Flowrate (CTF): 12.5 gpm/cartridge

Cartridges = Q x (449 gpm/cfs) / CTF
 # Cartridges = 0.048685 (round up)
 # Cartridges Required = 1
 Treatment Flow Rate Capacity = 0.02784 cfs

TABLE 1

NO.	ACTIVITIES FOR MEDIA FILTERS	
	MAINTENANCE TASK	FREQUENCY OF TASK
1	INSPECT FOR STANDING WATER, SEDIMENT, TRASH AND DEBRIS.	MONTHLY DURING RAINY SEASON
2	REMOVE ACCUMULATED TRASH AND DEBRIS IN THE UNIT DURING ROUTINE INSPECTIONS.	MONTHLY DURING RAINY SEASON, OR AS NEEDED AFTER STORM EVENTS
3	INSPECT TO ENSURE THAT THE FACILITY IS DRAINING COMPLETELY WITHIN FIVE DAYS AND PER MANUFACTURER'S SPECIFICATIONS.	ONCE DURING THE WET SEASON AFTER MAJOR STORM EVENT.
4	REPLACE THE MEDIA PER MANUFACTURER'S INSTRUCTIONS OR AS INDICATED BY THE CONDITION OF THE UNIT.	PER MANUFACTURER'S SPECIFICATIONS.
5	INSPECT MEDIA FILTERS USING THE ATTACHED INSPECTION CHECKLIST.	QUARTERLY OR AS NEEDED

SIZING FOR VOLUME BASED TREATMENT

DMA # 7
 A = 242 s.f.
 Impervious Area = 0 s.f.
 Pervious Pavement Area = 242 s.f.
 MAPsite = 15
 MAPgage = 13.9
 P6(gage) = 0.512 in
 P6(site) = P6(gage) x Correction Factor
 P6(site) = 0.55252 in

Cw = 0.858^{1/3} - 0.78i² + 0.774i + 0.04
 Cw = 0.0400000
 Regression Factor (a) a = 1.963 (48 hour draw down)

Po = a x Cw x P6(site)
 Po = 0.04338 in

Design Volume = Po x A x 14/12 in
 Design Volume = 0.8749 ft³

SELF RETAINING (PERVIOUS PAVEMENT)

Porosity of Rock*	Min. Storage Depth Required (in)	Pervious ≥ 1/2 Impervious**
0.40	0.11	Yes

Minimum Storage Depth = Design Volume (c.f.) / Pervious Pavement Area (s.f.) / rock porosity x 12 in/1 ft

* Porosity of Class II Permeable = 0.4 based on SCVUURP training.
 ** If value = "No" increase size of pervious pavement.

SDP SUBMITTAL

SHEET TITLE:
 MEDIA FILTER DETAILS

SHEET NO.:

C405

ALMADEN CORNER HOTEL

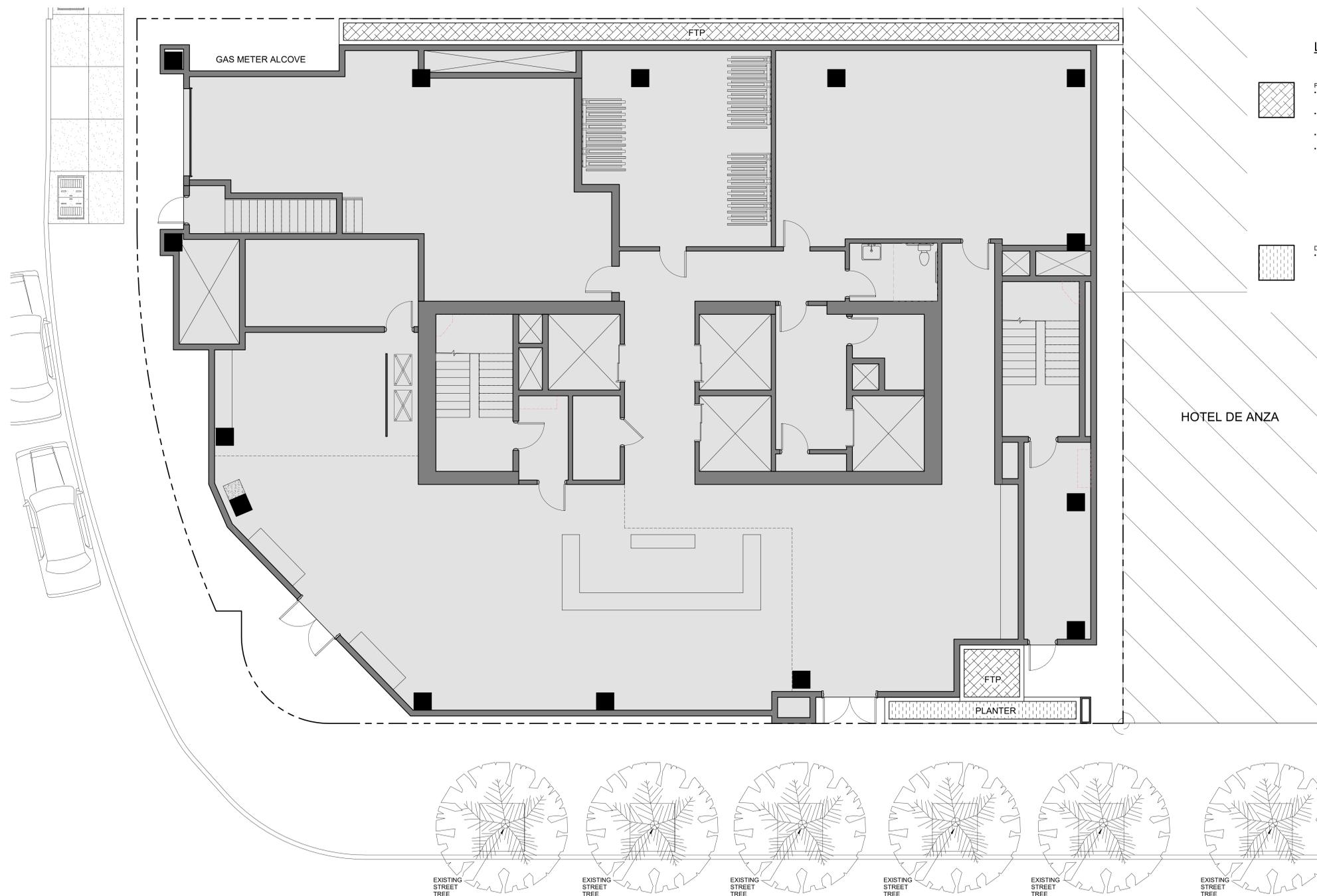
OWNER:
 KT URBAN

ADDRESS:
 8 N ALMADEN BLVD
 SAN JOSE, CA 95110

PROJECT NO.: 1847
 DRAWING: P65
 DATE: 01/30/2019
 SDP SUBMITTAL

REVISION: DESCRIPTION:
 6/10/19 REVISED PER CITY STANDARDS

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LANDSCAPE LEGEND

-  FLOW THROUGH PLANTERS:
 - PLANTING SPECIES TO BE SELECTED PER 2016 C3 STORMWATER HANDBOOK AS NOTED IN TABLE D-1 AS ACCEPTABLE FOR FLOW-THROUGH PLANTERS.
 - AVOID SHRUBBY SPECIES FOR ABOVE-GROUND FLOORS.
 - FLOW THROUGH PLANTERS FOR EASE OF MAINTENANCE AND INSPECTION ACCESS
 - PLACE 3" OF COMPOSTED, NON-FLOATABLE MULCH IN AREAS BETWEEN STORMWATER PLANTINGS.
 - FLOW-THROUGH PLANTER BOXES ONLY TO CONTAIN SPECIES IN THE "BASIN" PLANTING ZONE:
 1. SANTA BARBARA SEDGE
 2. BERKELEY SEDGE
 3. DUNE SEDGE
 4. LARGE CAPE RUSH
 5. SMALL CAPE RUSH
 6. CALIFORNIA GREY RUSH
 7. NEW ZEALAND WIND GRASS
 8. PURPLE NEEDLEGRASS
 9. KURAPIA
 10. BIOFILTRATION SOD
-  DECORATIVE PLANTERS
 - PLANTING SPECIES TO BE SELECTED PER DOWNTOWN STREETScape MASTER PLAN GUIDELINES.

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

PROJECT NO.: 17121
 DRAWN: PM
 DATE: 11/21/2019
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 REVISION: DESCRIPTION:

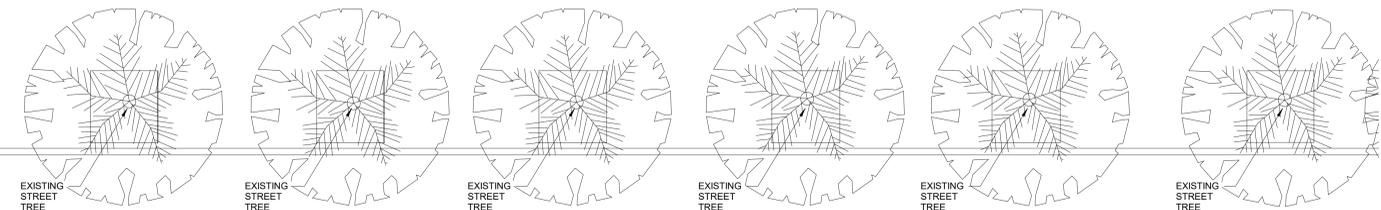
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SHEET TITLE:
LANDSCAPE PLAN - LEVEL 1

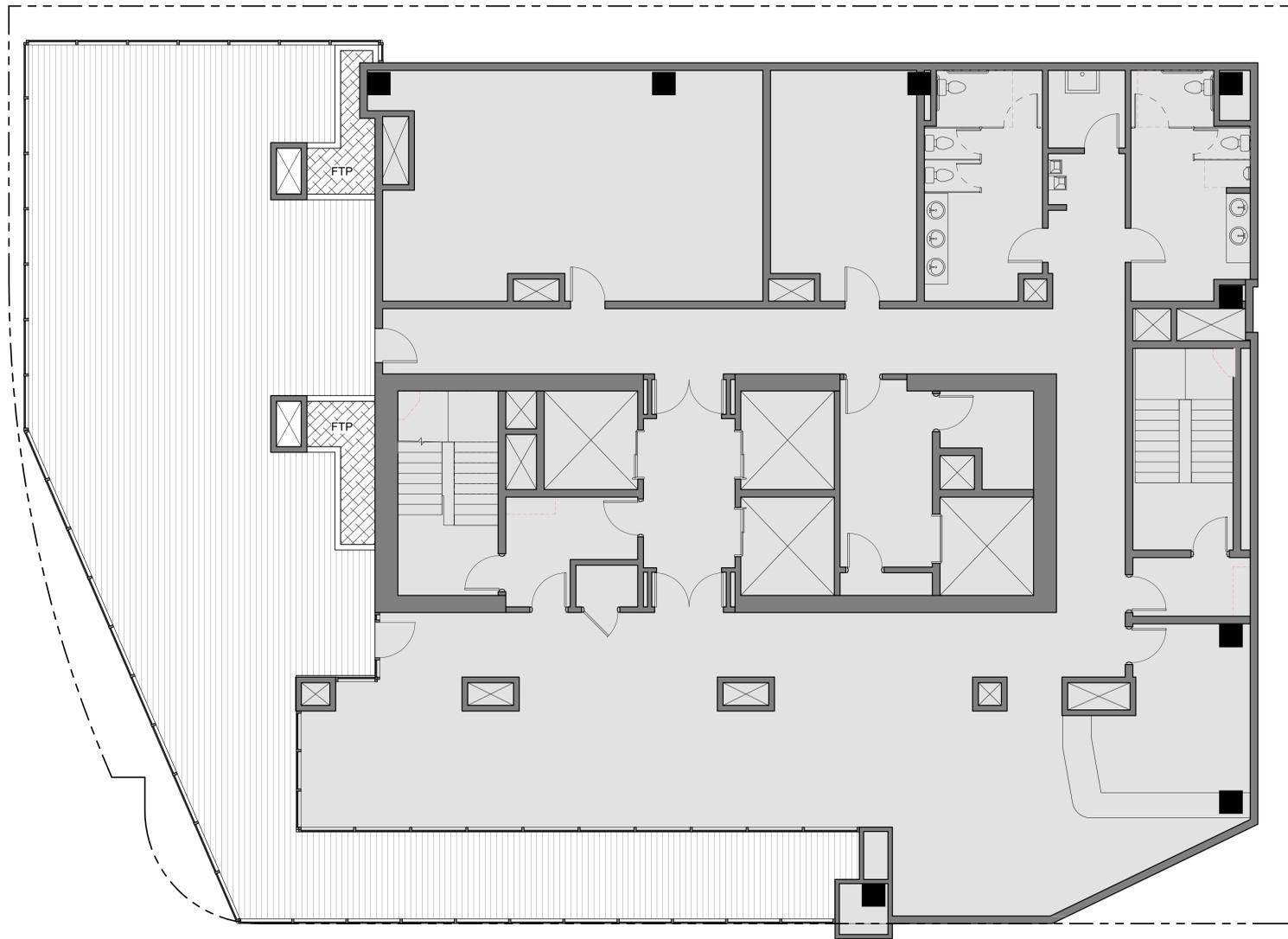
SHEET NO.:
L101

1 LP - 3/16 - L01
 L101 3/16" = 1'-0"



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11/25/2019 2:37:52 PM



LANDSCAPE LEGEND



- FLOW THROUGH PLANTERS:**
- PLANTING SPECIES TO BE SELECTED PER 2016 C3 STORMWATER HANDBOOK AS NOTED IN TABLE D-1 AS ACCEPTABLE FOR FLOW-THROUGH PLANTERS.
 - AVOID SHRUBBY SPECIES FOR ABOVE-GROUND FLOORS.
 - FLOW-THROUGH PLANTERS FOR EASE OF MAINTENANCE AND INSPECTION ACCESS.
 - PLACE 3" OF COMPOSTED, NON-FLOATABLE MULCH IN AREAS BETWEEN STORMWATER PLANTINGS.
 - FLOW-THROUGH PLANTER BOXES ONLY TO CONTAIN SPECIES IN THE "BASIN" PLANTING ZONE:
- SANTA BARBARA SEDGE
 - BERKELEY SEDGE
 - DUNE SEDGE
 - LARGE CAPE RUSH
 - SMALL CAPE RUSH
 - CALIFORNIA GREY RUSH
 - NEW ZEALAND WIND GRASS
 - PURPLE NEEDLEGRASS
 - KURAPIA
 - BIOFILTRATION SOD



- DECORATIVE PLANTERS**
- PLANTING SPECIES TO BE SELECTED PER DOWNTOWN STREETScape MASTER PLAN GUIDELINES.

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

PROJECT NO.: 17121
 DRAWN: PM
 DATE: 11/21/2019
 H18-038 SITE DEVELOPMENT PERMIT
 REVISION: DESCRIPTION:

1
 L119 LP - 3/16 - L19
 3/16" = 1'-0"



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SHEET TITLE:
LANDSCAPE PLAN - LEVEL 19

SHEET NO.:
L119

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

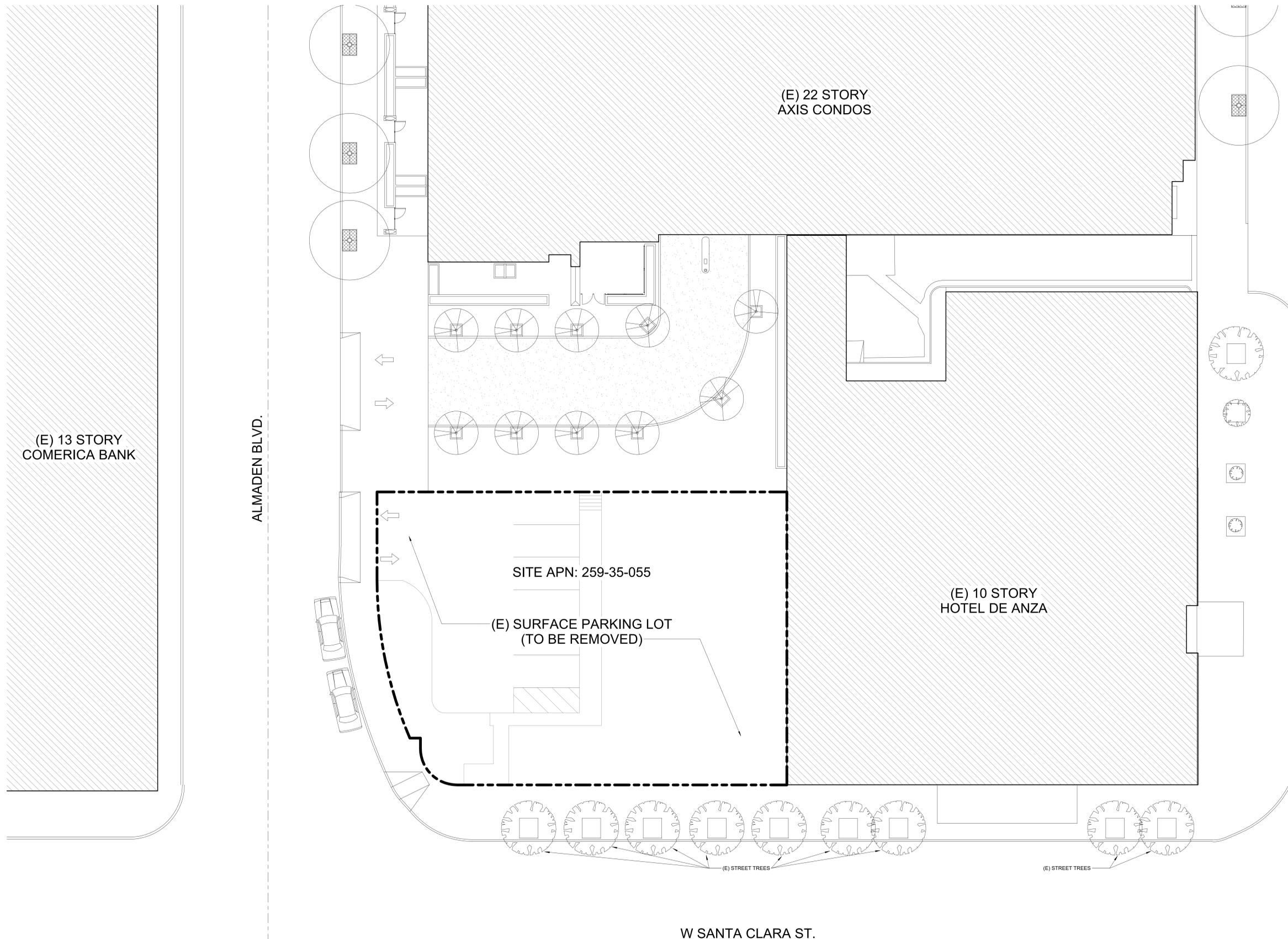
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 DRAWN: PM
 DATE: 11/21/2019
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H18-038 SITE DEVELOPMENT PERMIT

SHEET TITLE:
EXISTING SITE

SHEET NO.:
A100



**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

PROJECT NO.: 17121
 DRAWN: PM
 DATE: 11/21/2019
 H18-038 SITE DEVELOPMENT PERMIT
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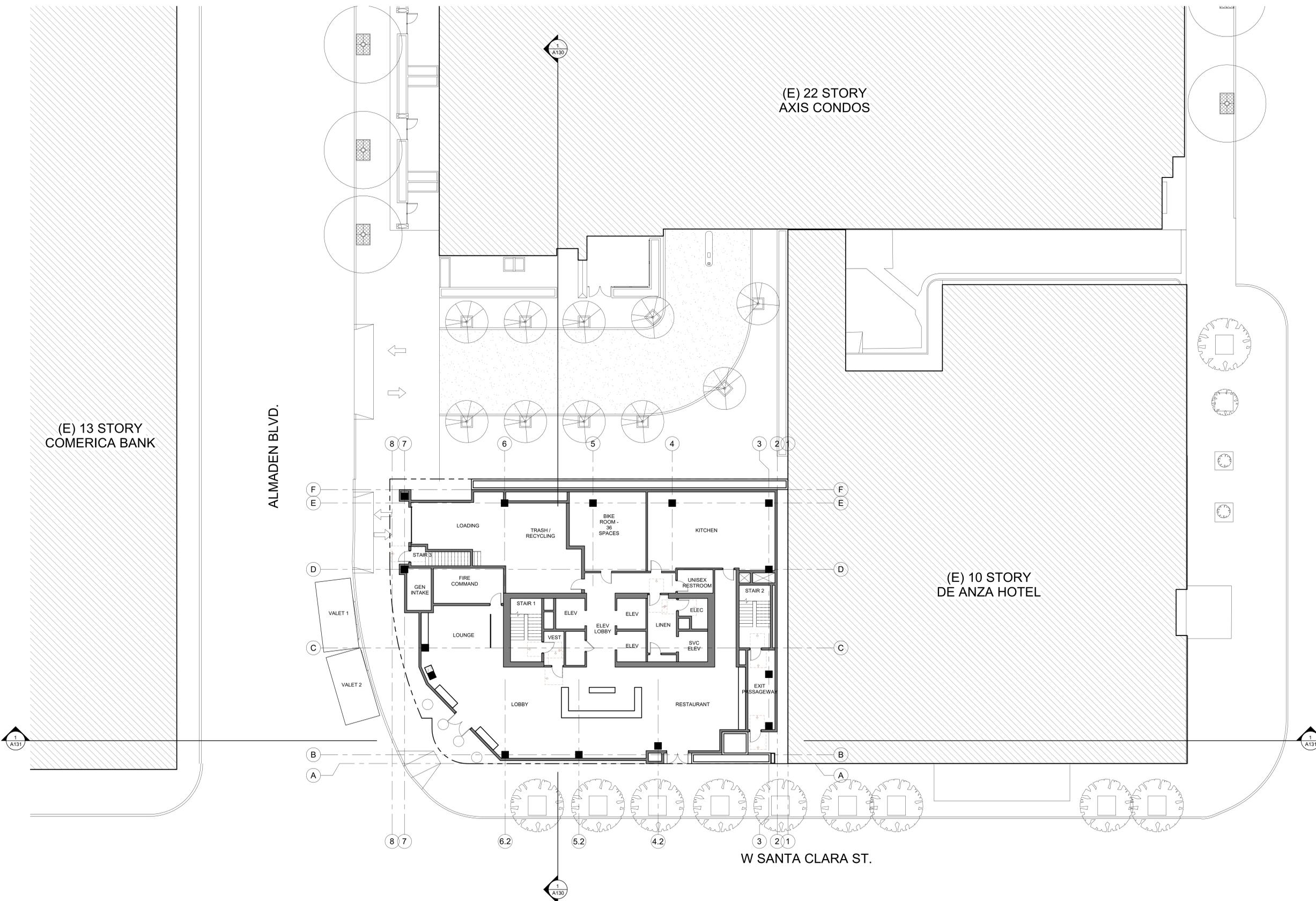
SHEET TITLE:
SITE PLAN

SHEET NO.:

A101

NOTRE DAME AVE.

H18-038 SITE DEVELOPMENT PERMIT



0 2' 4' 8' 16'



1 SITE PLAN
 A101 3/32" = 1'-0"

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

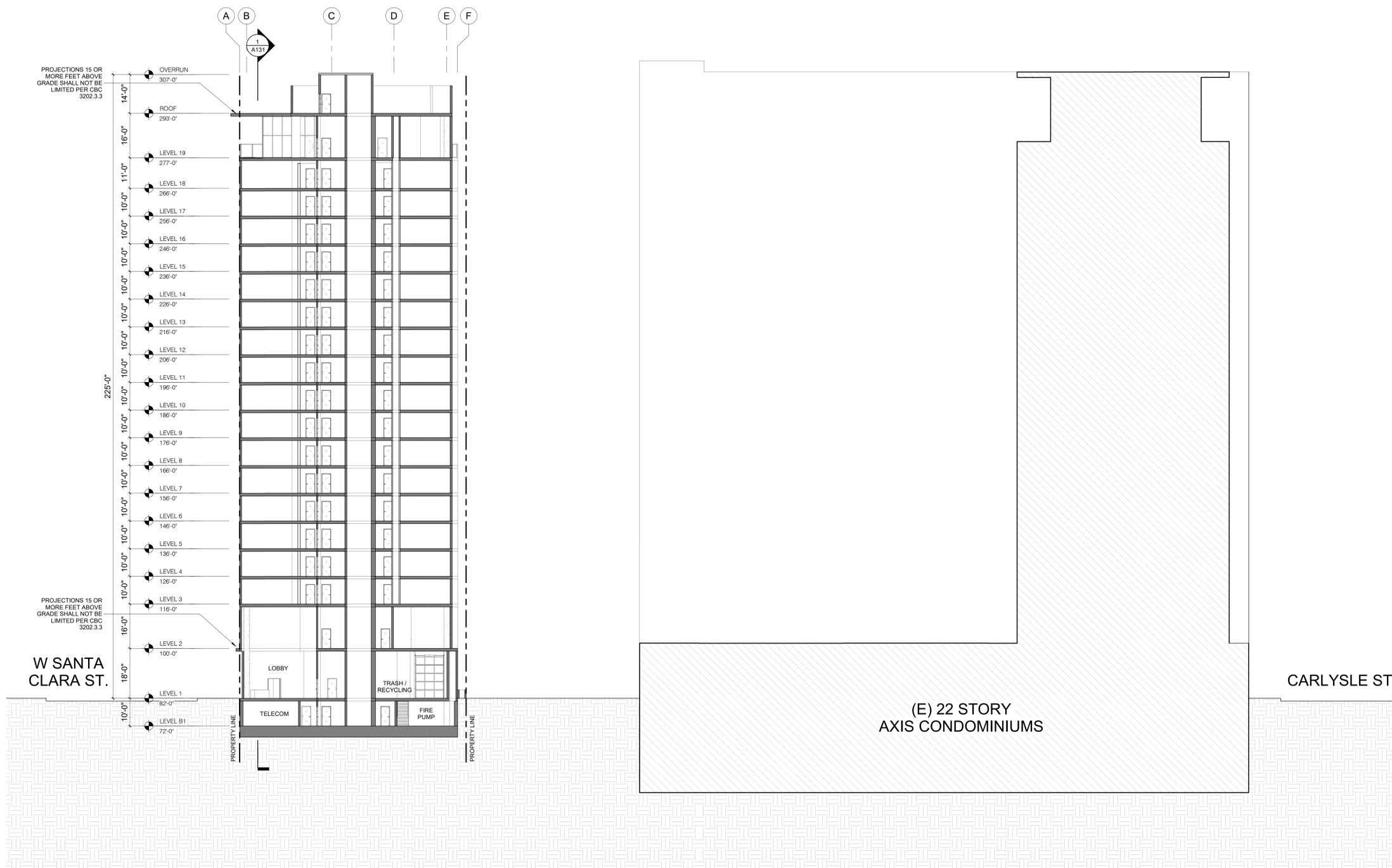
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 DRAWN: PM
 DATE: 11/21/2019
 H18-038 SITE DEVELOPMENT PERMIT
 REVISION: DESCRIPTION:

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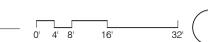
H18-038 SITE DEVELOPMENT PERMIT

SHEET TITLE:
SITE SECTION

SHEET NO.:
A130



1 SITE SECTION EAST
 A130 1/16" = 1'-0"



C:\Revit_Local\17121-Almaden Corner KTU HOTEL_CENTRAL_perm_a.rvt

11/25/2019 2:36:04 PM

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

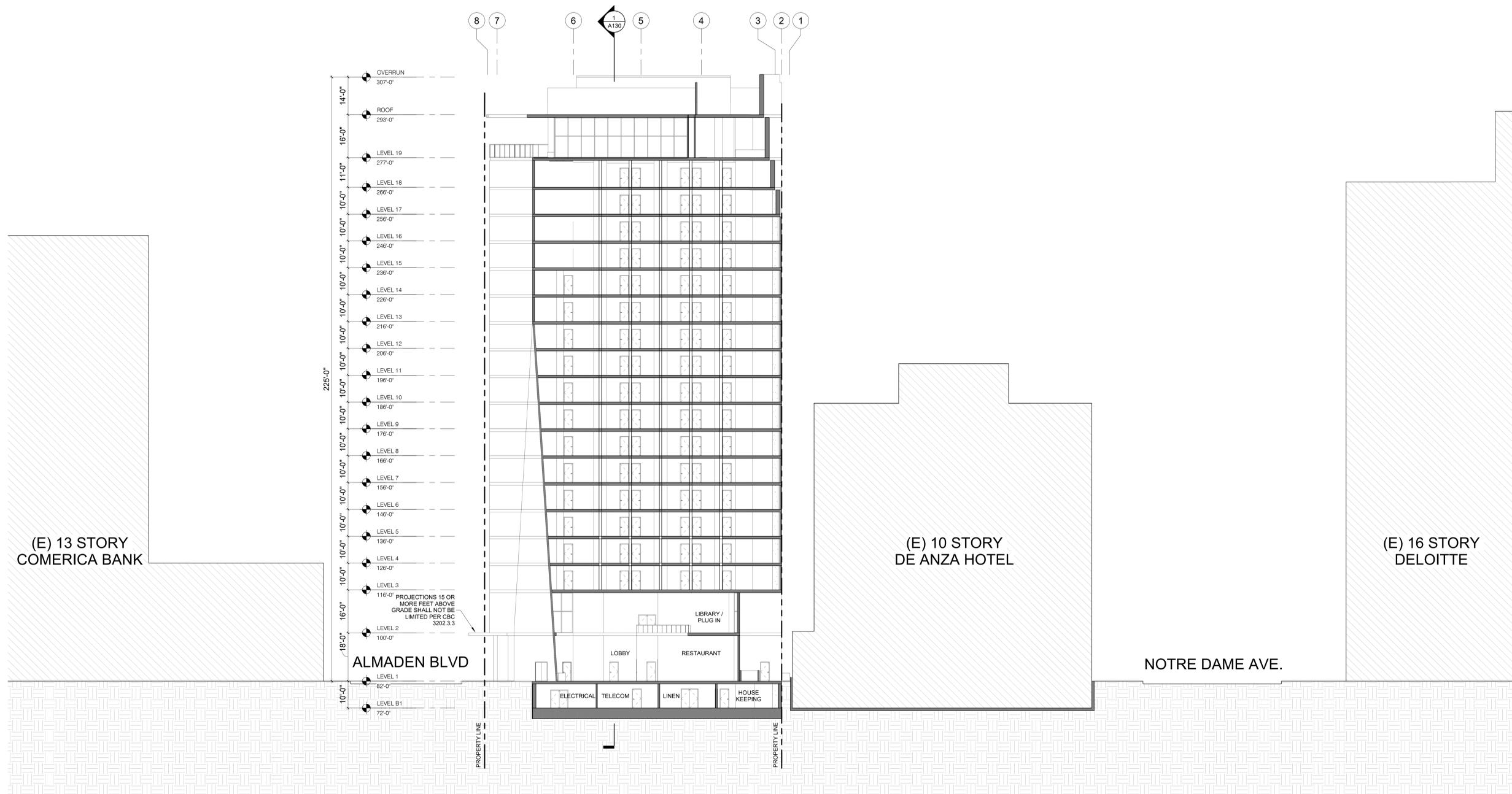
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 DATE: 11/21/2019
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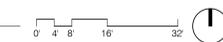
H18-038 SITE DEVELOPMENT PERMIT

SHEET TITLE:
SITE SECTION

SHEET NO.:
A131



1 SITE SECTION NORTH
 A131 1/16" = 1'-0"



C:\Revit_Local\17121-Almaden Corner KTU HOTEL_CENTRAL_perm_a.rvt

11/25/2019 2:36:09 PM

ALMADEN CORNER HOTEL

OWNER:
KT URBAN

ADDRESS:
8 N ALMADEN BLVD
SAN JOSE, CA 95110
H18-038

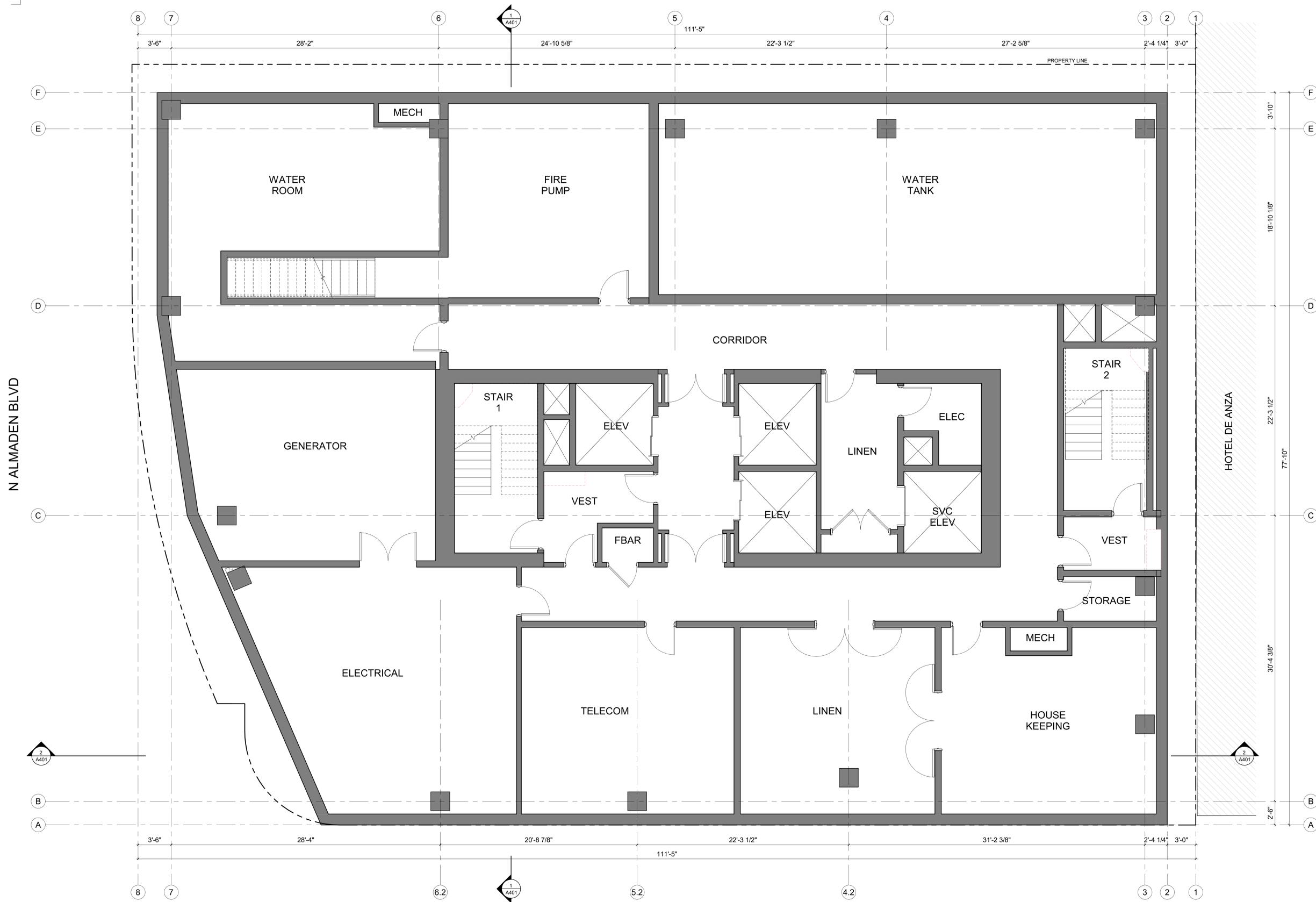
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DRAWN: PM
DATE: 11/21/2019
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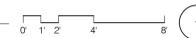
H18-038 SITE DEVELOPMENT PERMIT

SHEET TITLE:
FLOOR PLAN - LEVEL B1

SHEET NO.:
A2B1



1 FLOOR PLAN - LEVEL B1
A2B1 1/4" = 1'-0"



W SANTA CLARA ST

ALMADEN CORNER HOTEL

OWNER:
KT URBAN

ADDRESS:
8 N ALMADEN BLVD
SAN JOSE, CA 95110
H18-038

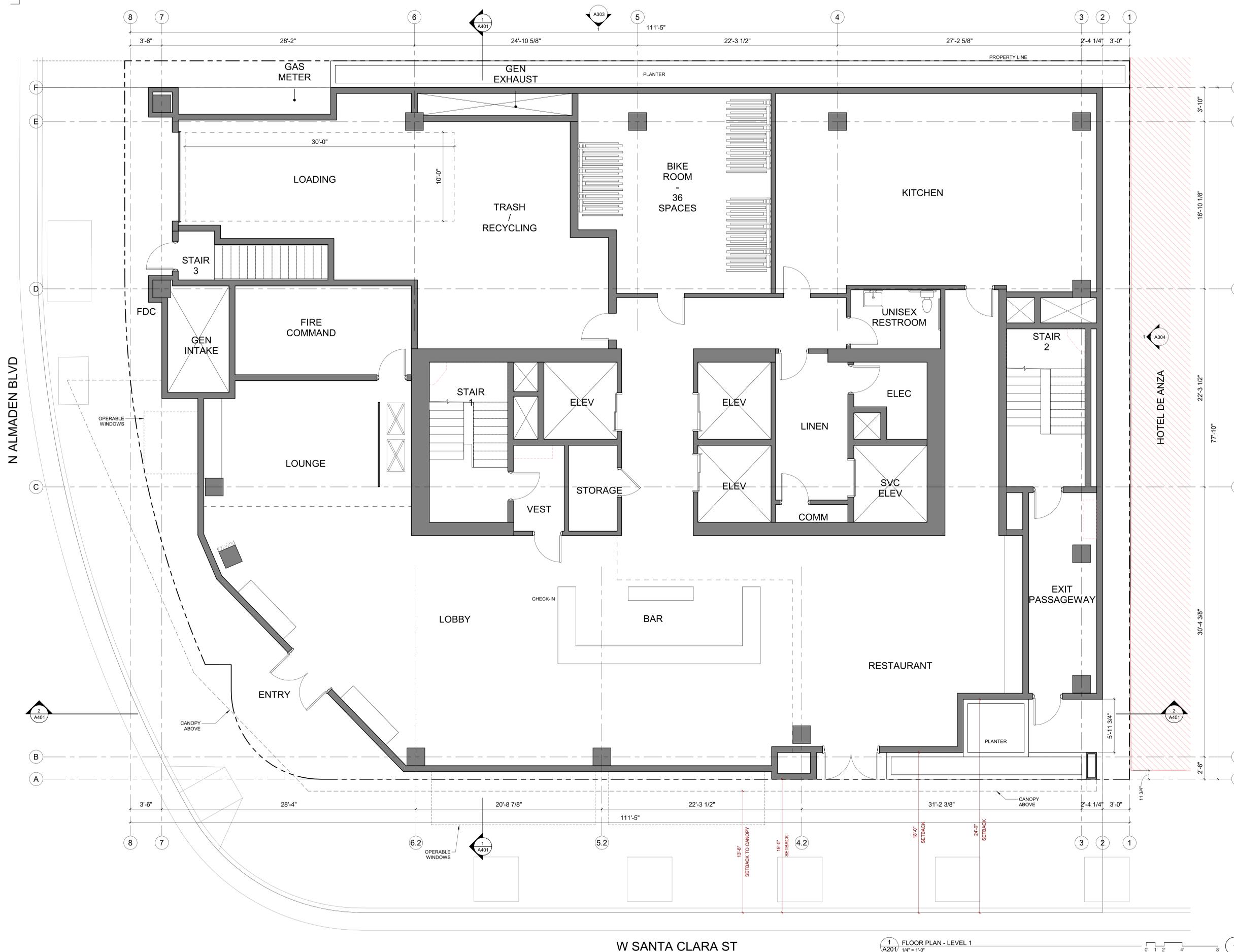
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DRAWN: PM
DATE: 11/21/2019
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REVISION: DESCRIPTION:

H18-038 SITE DEVELOPMENT PERMIT

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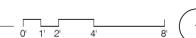
SHEET TITLE:
FLOOR PLAN - LEVEL 1

SHEET NO.:
A201



W SANTA CLARA ST

1 FLOOR PLAN - LEVEL 1
A201 1/4" = 1'-0"



ALMADEN CORNER HOTEL

OWNER:
KT URBAN

ADDRESS:
8 N ALMADEN BLVD
SAN JOSE, CA 95110
H18-038

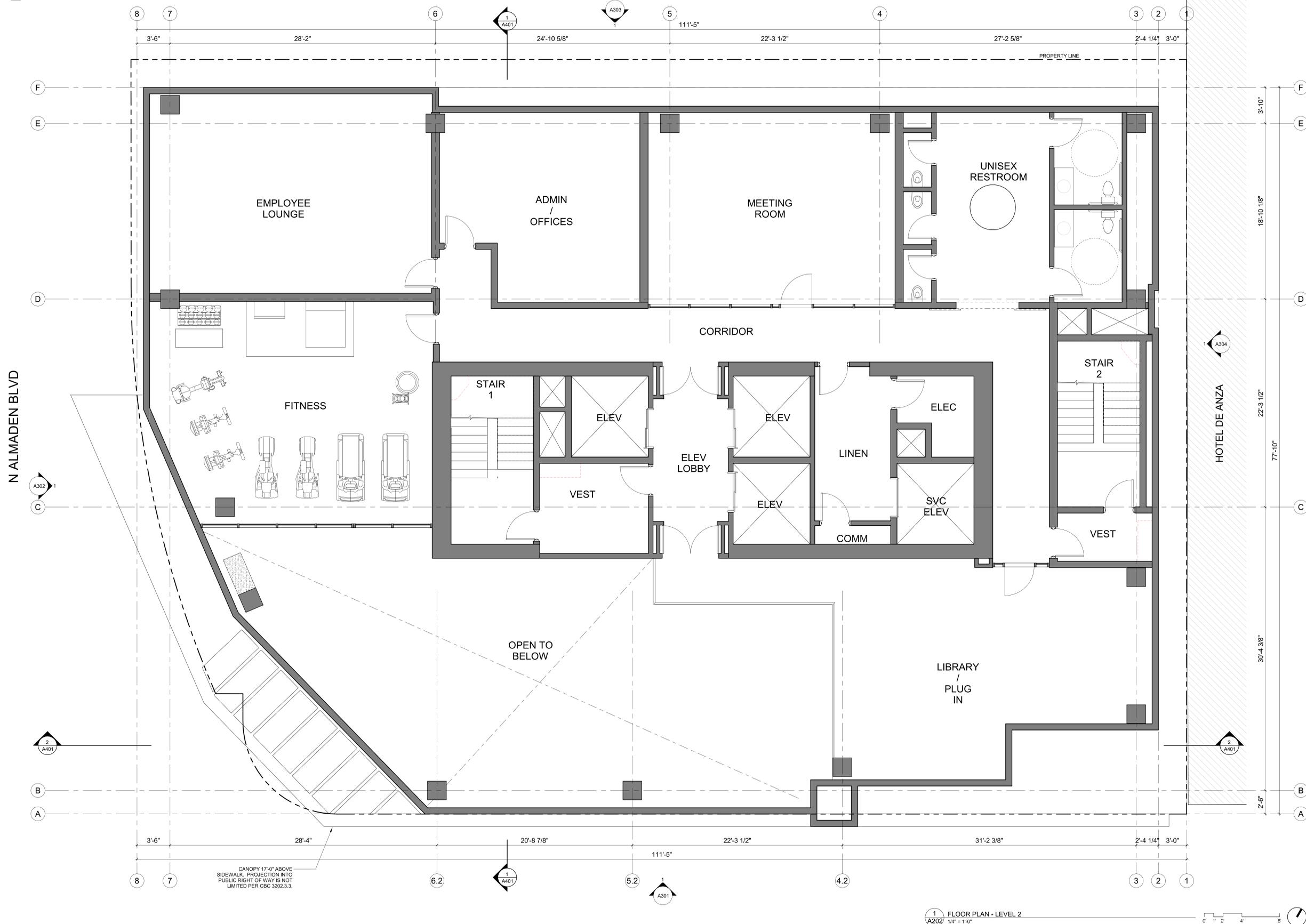
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DATE: 11/21/2019
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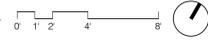
H18-038 SITE DEVELOPMENT PERMIT

SHEET TITLE:
FLOOR PLAN - LEVEL 2

SHEET NO.:
A202



1 FLOOR PLAN - LEVEL 2
A202 1/4" = 1'-0"



CANOPY 17'-0" ABOVE
SIDEWALK PROJECTION INTO
PUBLIC RIGHT OF WAY IS NOT
LIMITED PER CBC 3202.3.3.

W SANTA CLARA ST

C:\Revit_Local\17121-Almaden Corner KTU HOTEL CENTRAL_perm_a.rvt

11/25/2019 2:36:11 PM

ALMADEN CORNER HOTEL

OWNER:
KT URBAN

ADDRESS:
8 N ALMADEN BLVD
SAN JOSE, CA 95110
H18-038

PROJECT NO.: 17121
DRAWN: PM
DATE: 11/21/2019
H18-038 SITE DEVELOPMENT PERMIT
REVISION: DESCRIPTION:

H18-038 SITE DEVELOPMENT PERMIT

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SHEET TITLE:
FLOOR PLAN - LEVEL 3 (TYPICAL FLOOR)

SHEET NO.:
A203



2 FLOOR PLAN - LEVEL 3
A203 1/4" = 1'-0"

W SANTA CLARA ST

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

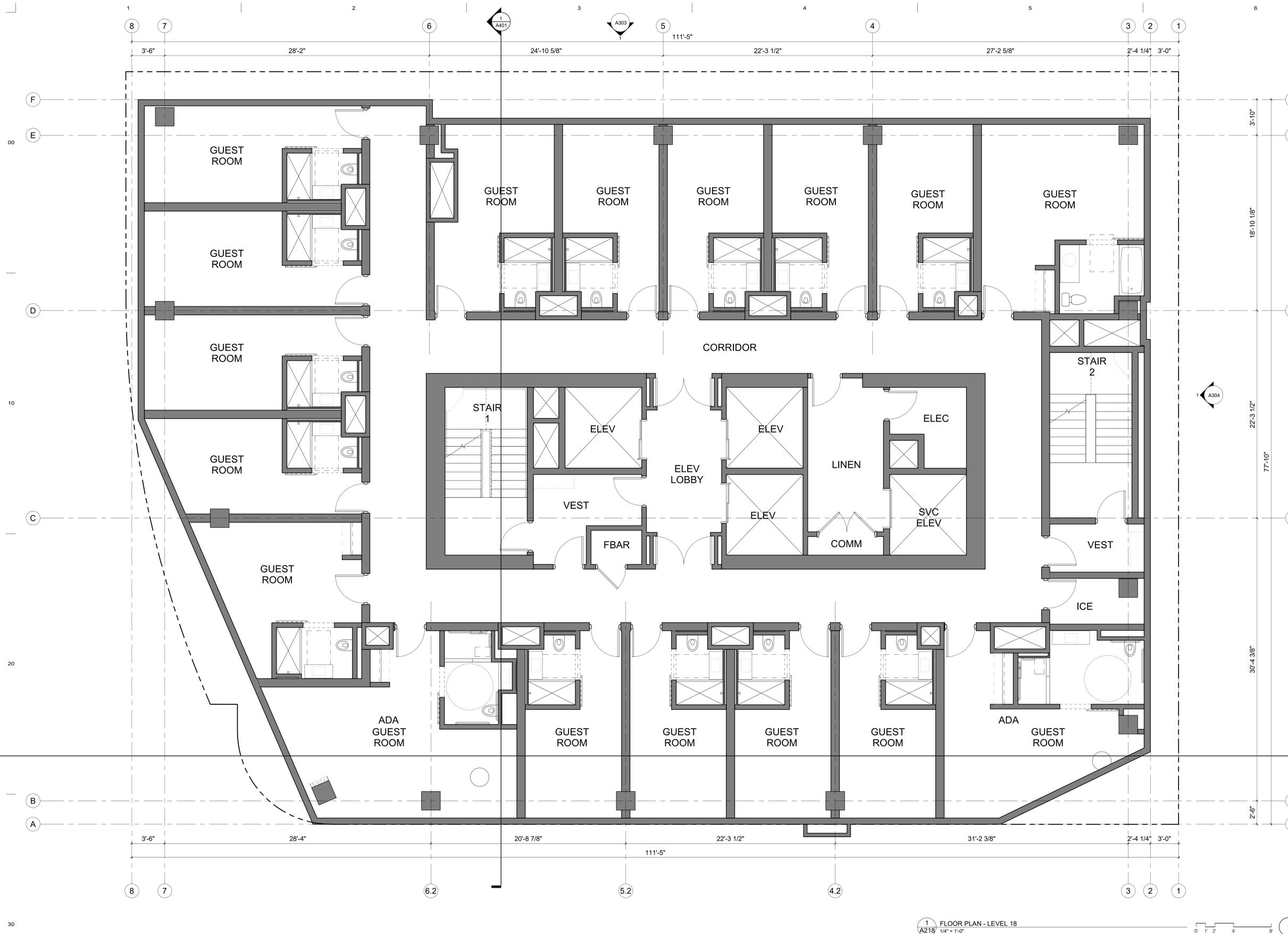
PROJECT NO.: 17121
 DRAWN: PM
 DATE: 11/21/2019
 H18-038 SITE DEVELOPMENT PERMIT
 REVISION: DESCRIPTION:

H18-038 SITE DEVELOPMENT PERMIT

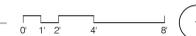
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SHEET TITLE:
FLOOR PLAN - LEVEL 18

SHEET NO.:
A218



1 FLOOR PLAN - LEVEL 18
A218 1/4" = 1'-0"



ALMADEN CORNER HOTEL

OWNER:
 KT URBAN

ADDRESS:
 8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038

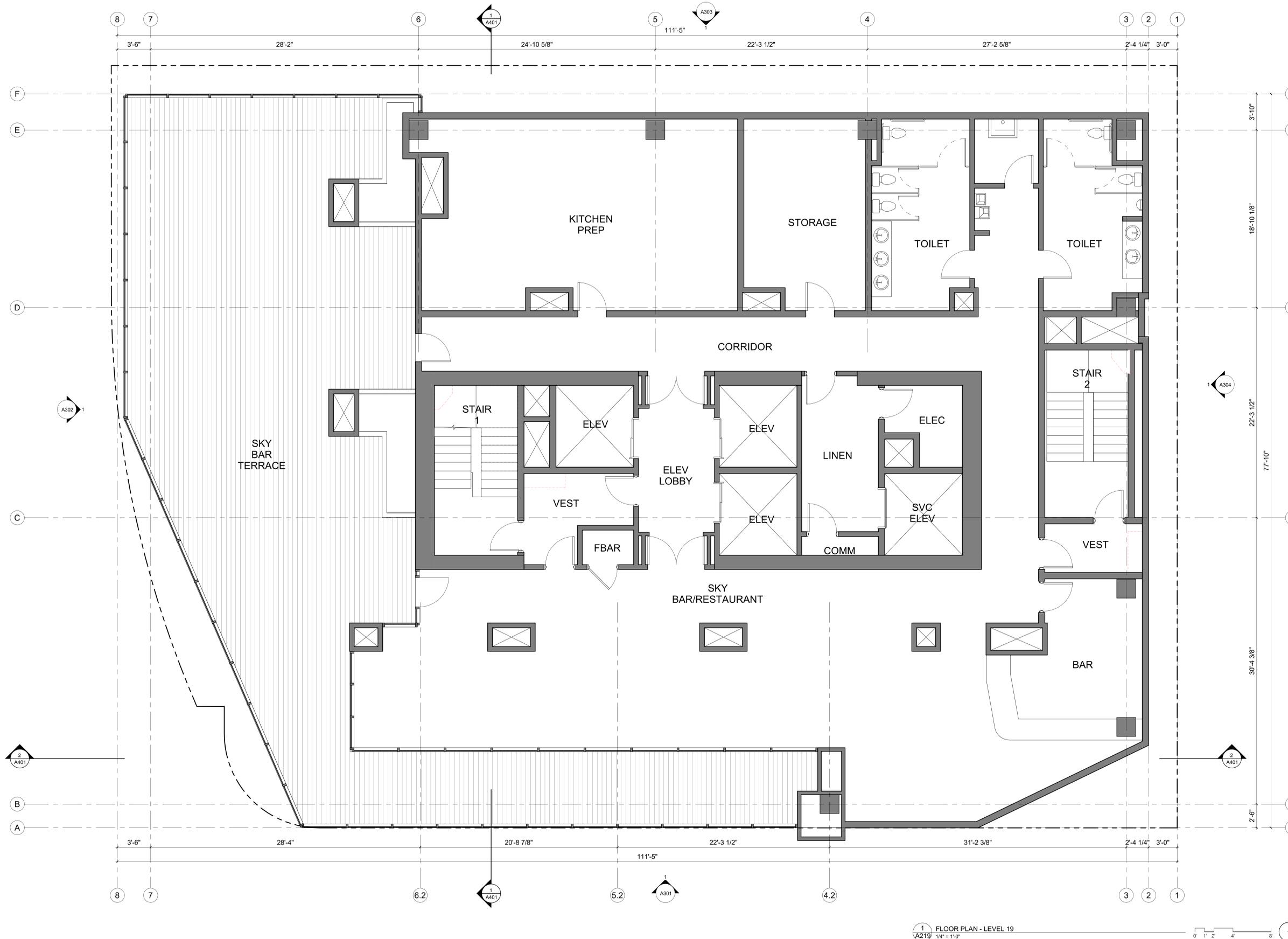
PROJECT NO.: 17121
 DRAWN: PM
 DATE: 11/21/2019
 H18-038 SITE DEVELOPMENT PERMIT
 REVISION: DESCRIPTION:

H18-038 SITE DEVELOPMENT PERMIT

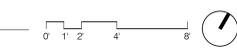
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SHEET TITLE:
 FLOOR PLAN - LEVEL 19 / ROOF
 DECK

SHEET NO.:
A219



1 FLOOR PLAN - LEVEL 19
 A219 1/4" = 1'-0"



C:\Revit_Local\17121-Almaden Corner KTU HOTEL CENTRAL_pcm.a.rvt
 11/25/2019 2:36:15 PM

ALMADEN CORNER HOTEL

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

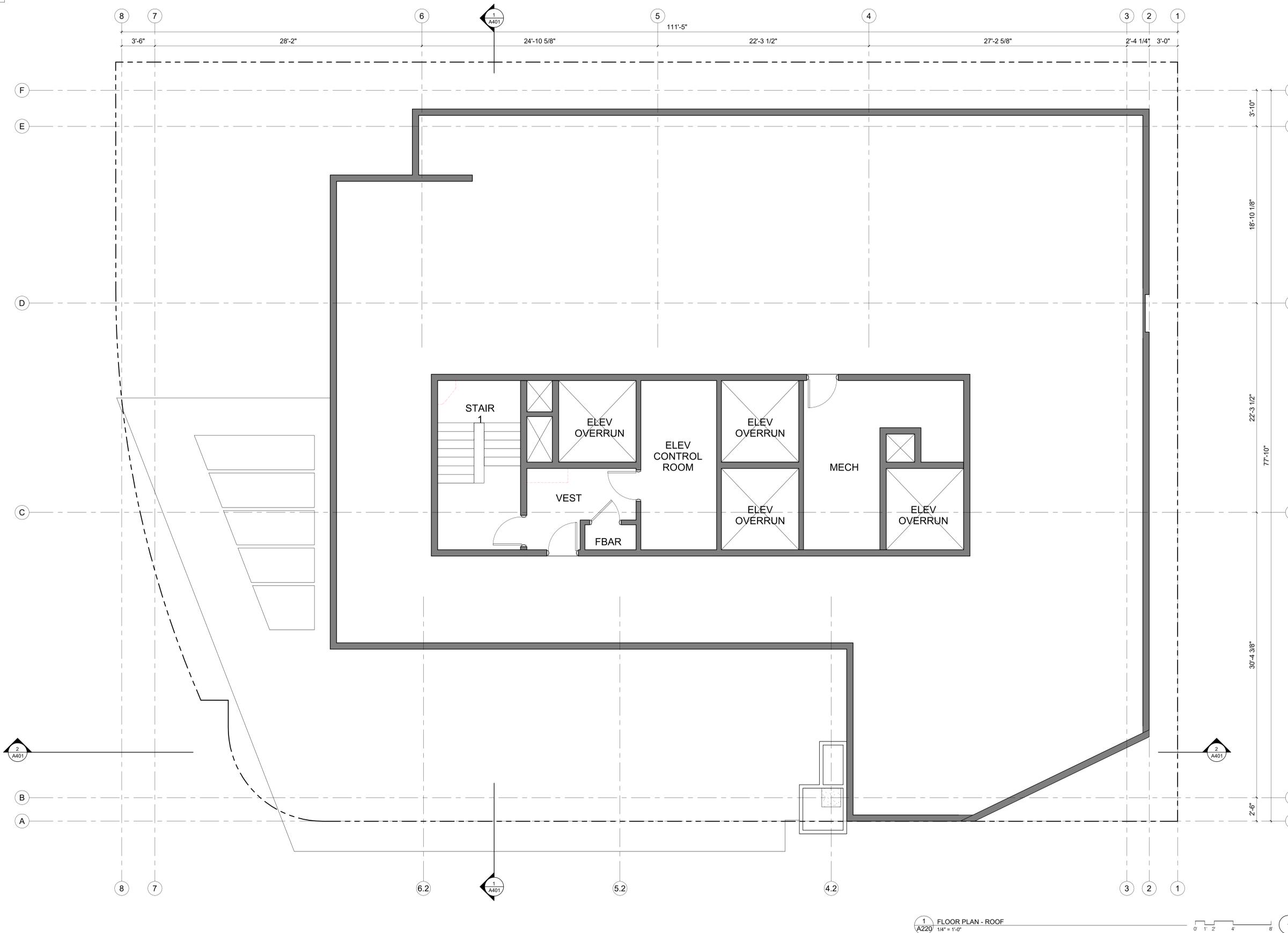
PROJECT NO.: 17121
 DRAWN: PM
 DATE: 11/21/2019
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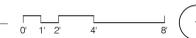
H18-038 SITE DEVELOPMENT PERMIT

SHEET TITLE:
ROOF PLAN

SHEET NO.:
A220

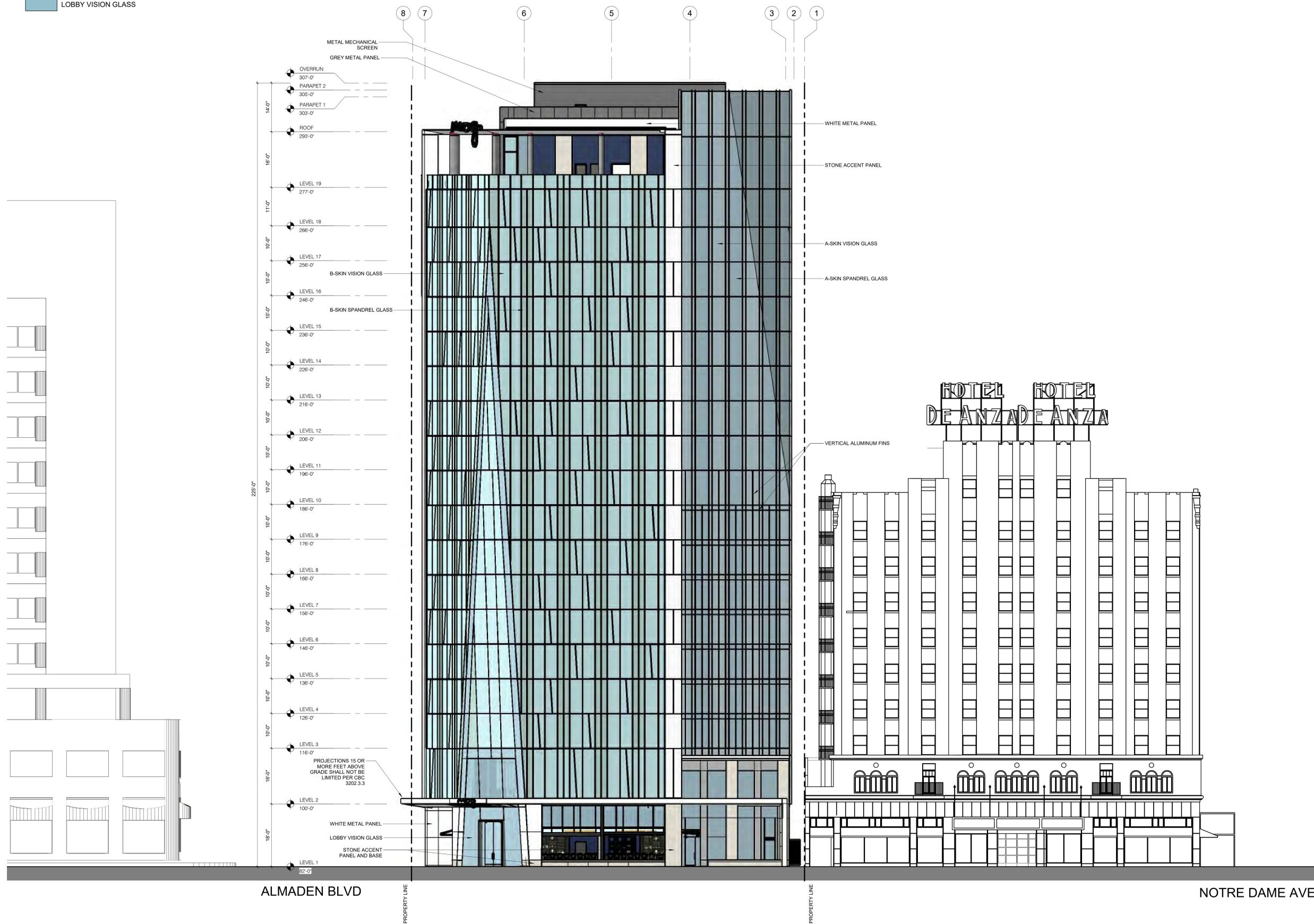


1 FLOOR PLAN - ROOF
 A220 1/4" = 1'-0"



MATERIAL LEGEND

- GL-1 / GL-2 A-SKIN VISION / SPANDREL
- GL-3 / GL-4 B-SKIN VISION / SPANDREL
- GL-5 LOBBY VISION GLASS
- ST-1 STONE ACCENT PANEL
- MTL-1 WHITE METAL PANEL
- MTL-2 GREY METAL PANEL
- MTL-3 GREY MECHANICAL SCREEN
- MTL-4 BLUE ACCENT METAL SCREEN



**ALMADEN
 CORNER
 HOTEL**

OWNER:
 KT URBAN

ADDRESS:
 8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038

PROJECT NO.: 17121
 DRAWN: PM
 DATE: 11/21/2019
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H18-038 SITE DEVELOPMENT PERMIT

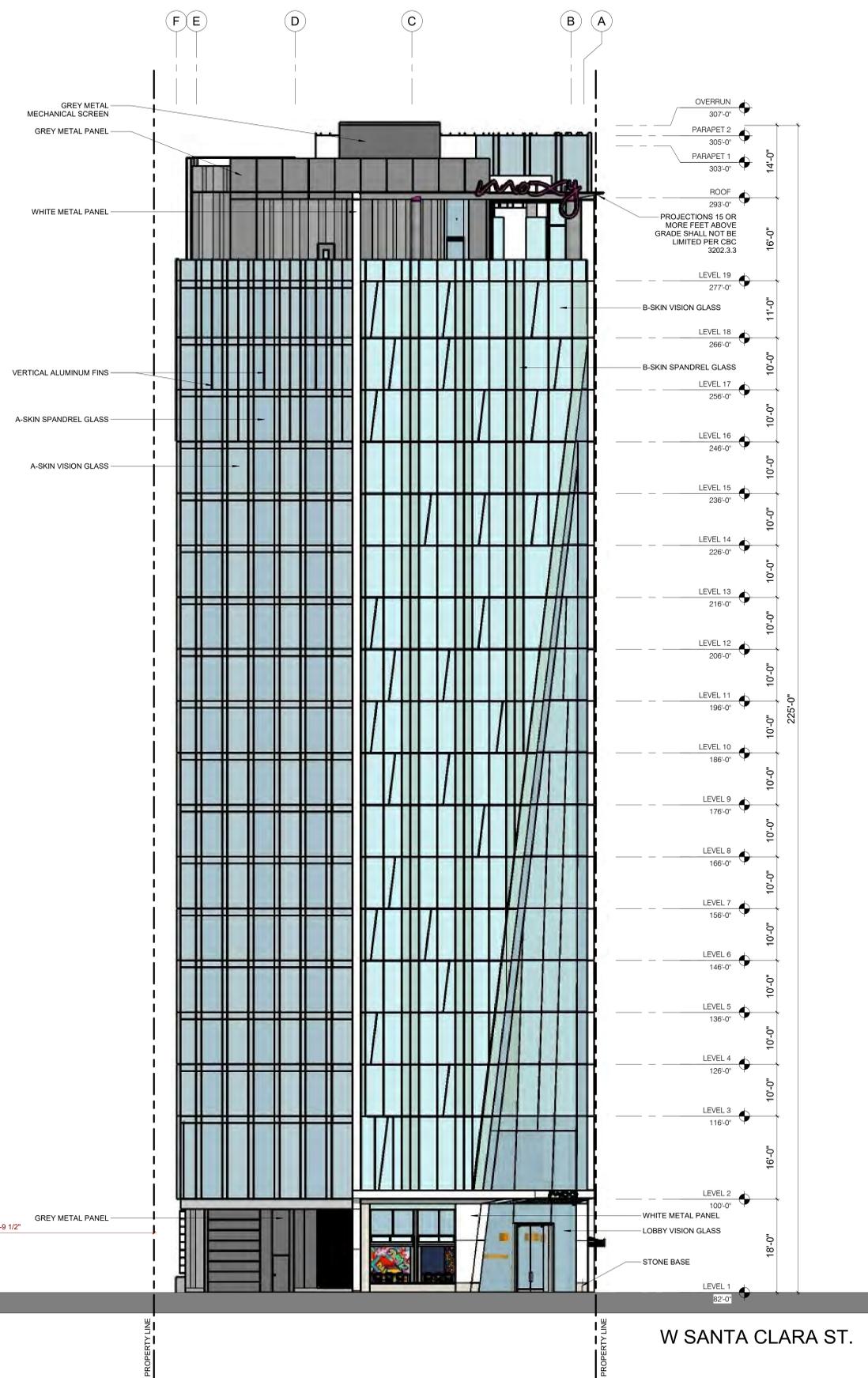
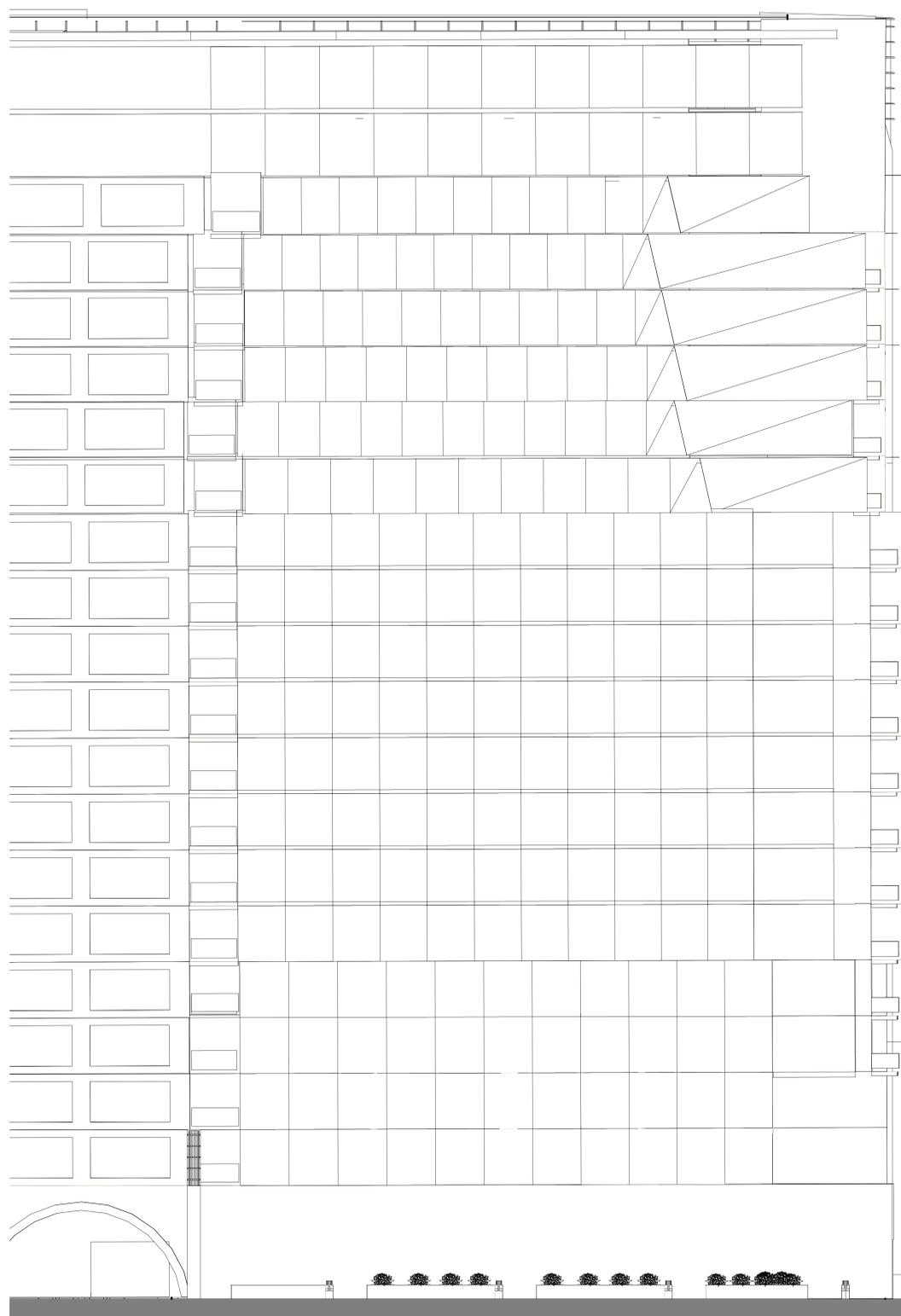
SHEET TITLE:
 EXTERIOR ELEVATIONS - SOUTH
 (SANTA CLARA ST)

SHEET NO.:

A301

MATERIAL LEGEND

-  GL-1 / GL-2
A-SKIN VISION / SPANDREL
-  GL-3 / GL-4
B-SKIN VISION / SPANDREL
-  GL-5
LOBBY VISION GLASS
-  ST-1
STONE ACCENT PANEL
-  MTL-1
WHITE METAL PANEL
-  MTL-2
GREY METAL PANEL
-  MTL-3
GREY MECHANICAL SCREEN
-  MTL-4
BLUE ACCENT METAL SCREEN



ALMADEN CORNER HOTEL

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

PROJECT NO.: 17121
 DRAWN: PM
 DATE: 11/21/2019
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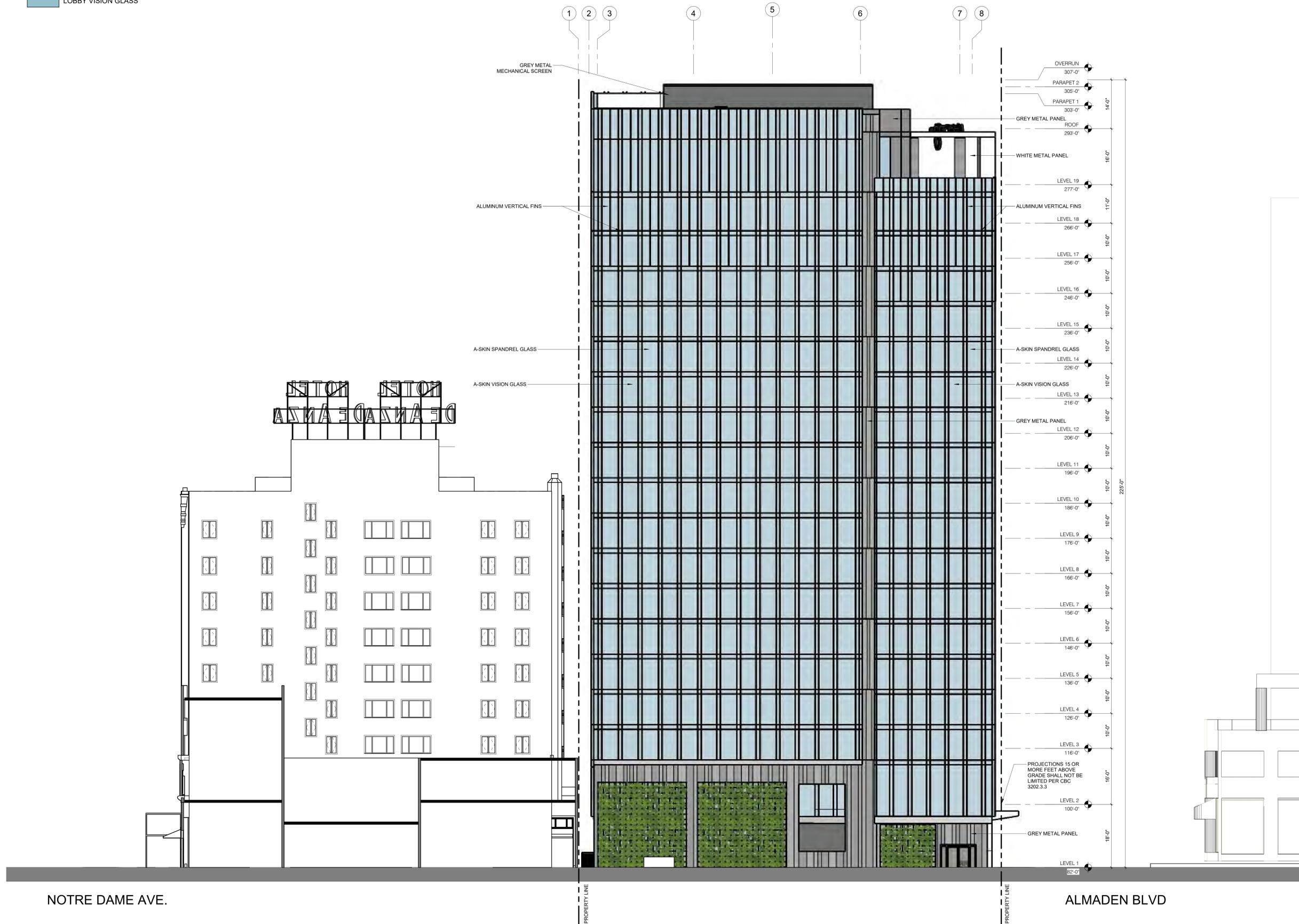
SHEET TITLE:
**EXTERIOR ELEVATION - WEST
 (ALMADEN AVE)**

SHEET NO.:

A302

MATERIAL LEGEND

-  GL-1 / GL-2
A-SKIN VISION / SPANDREL
-  GL-3 / GL-4
B-SKIN VISION / SPANDREL
-  GL-5
LOBBY VISION GLASS
-  ST-1
STONE ACCENT PANEL
-  MTL-1
WHITE METAL PANEL
-  MTL-2
GREY METAL PANEL
-  MTL-3
GREY MECHANICAL SCREEN
-  MTL-4
BLUE ACCENT METAL SCREEN



**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

PROJECT NO.: 17121
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H18-038 SITE DEVELOPMENT PERMIT

SHEET TITLE:
EXTERIOR ELEVATION - NORTH

SHEET NO.:
A303

MATERIAL LEGEND

- GL-1 / GL-2
A-SKIN VISION / SPANDREL
- GL-3 / GL-4
B-SKIN VISION / SPANDREL
- GL-5
LOBBY VISION GLASS

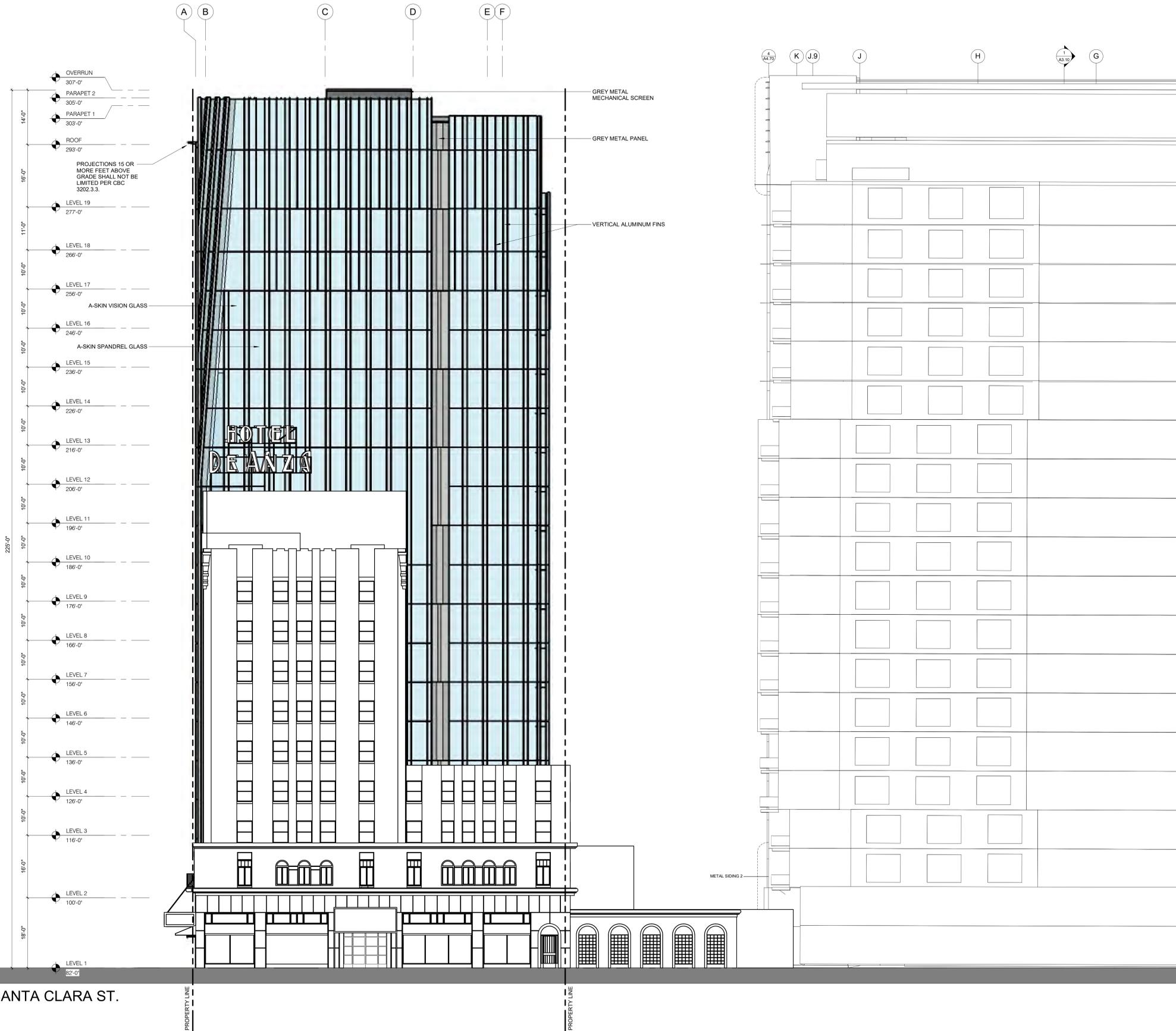
- ST-1
STONE ACCENT PANEL

- MTL-1
WHITE METAL PANEL

- MTL-2
GREY METAL PANEL

- MTL-3
GREY MECHANICAL SCREEN

- MTL-4
BLUE ACCENT METAL SCREEN



ALMADEN CORNER HOTEL

OWNER:
KT URBAN

ADDRESS:
8 N ALMADEN BLVD
SAN JOSE, CA 95110
H18-038

PROJECT NO.: 17121
DRAWN: PM
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H18-038 SITE DEVELOPMENT PERMIT

SHEET TITLE:
EXTERIOR ELEVATION - EAST

SHEET NO.:

A304

0 2 4 8 16

1 EAST ELEVATION
A304 3/32" = 1'-0"

**ALMADEN
 CORNER
 HOTEL**

OWNER:
KT URBAN

ADDRESS:
**8 N ALMADEN BLVD
 SAN JOSE, CA 95110
 H18-038**

PROJECT NO.: 17121
 DRAWN: PM
 DATE: 11/21/2019
 H18-038 SITE DEVELOPMENT PERMIT

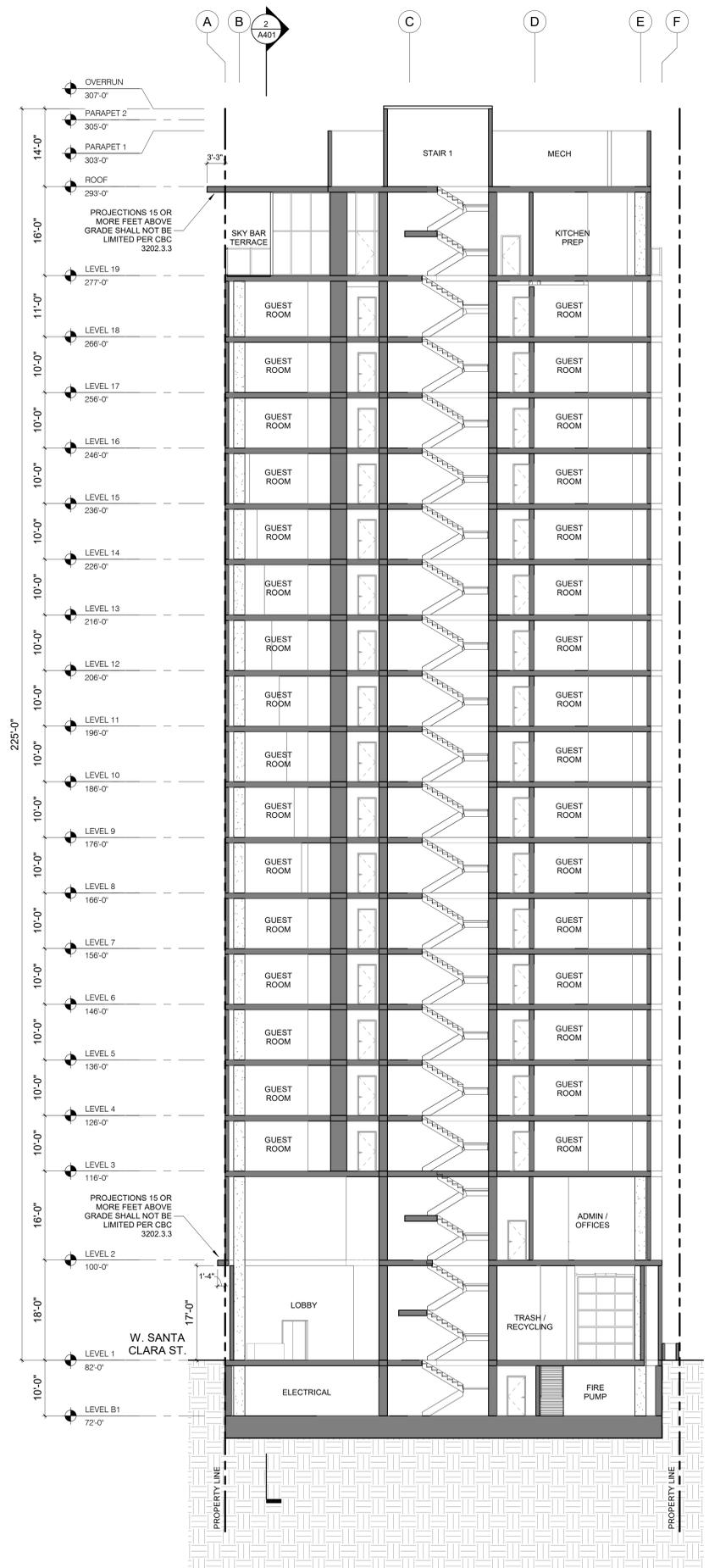
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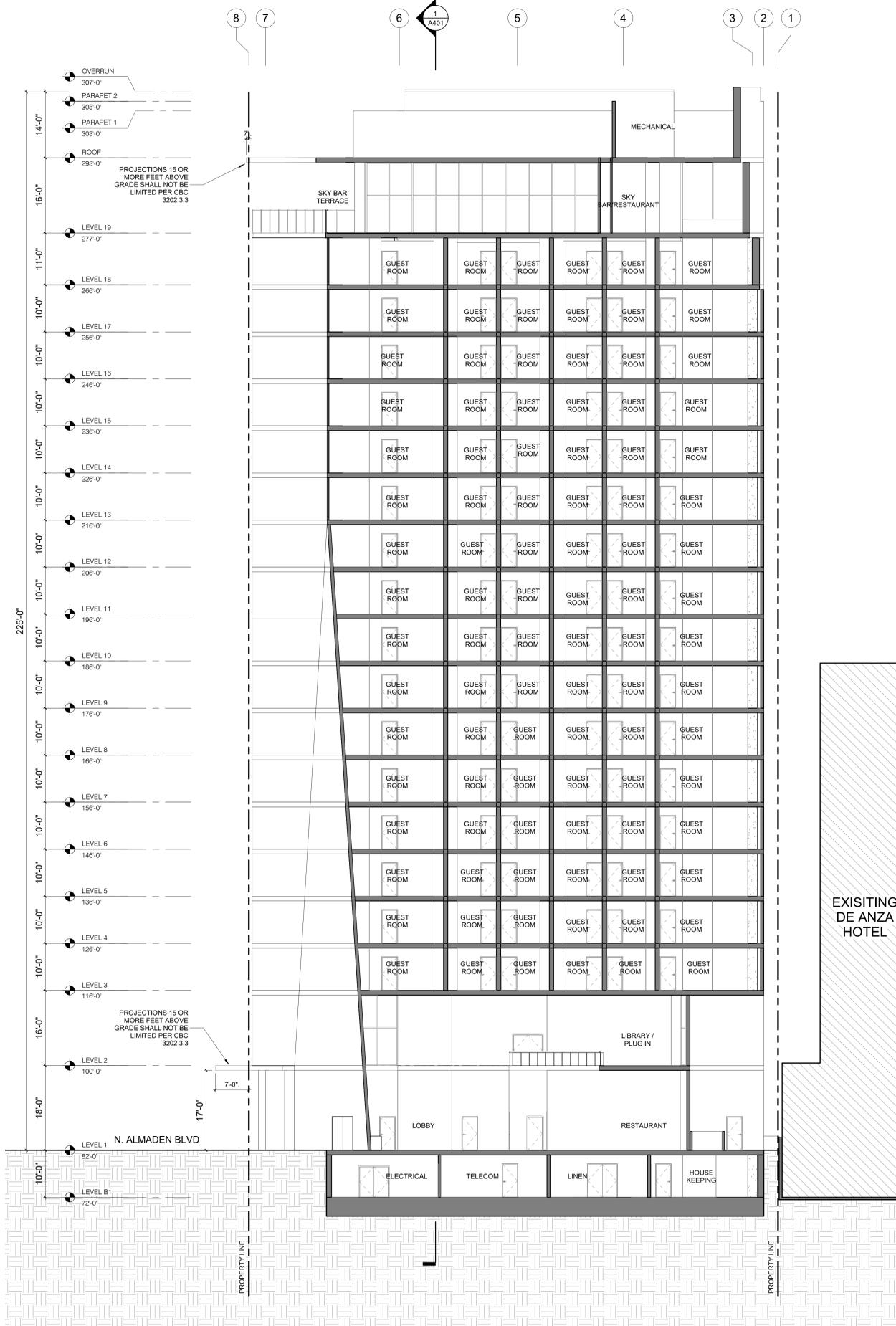
H18-038 SITE DEVELOPMENT PERMIT

SHEET TITLE:
BUILDING SECTIONS

SHEET NO.:
A401



1
A401
BUILDING SECTION EAST
 3/32" = 1'-0"



2
A401
BUILDING SECTION NORTH
 3/32" = 1'-0"



EXISTING DE ANZA HOTEL

From: Robert Del Rio [<mailto:rdelrio@hextrans.com>]

Sent: Tuesday, July 23, 2019 11:51 AM

To: Villanueva, Arlyn <Arlyn.Villanueva@sanjoseca.gov>; Do, Ryan <ryan.do@sanjoseca.gov>

Subject: Almaden Hotel Valet

Per our phone conversation, we provide the following in regard to the adequacy of valet parking for the Almaden Hotel development at 272 W. Santa Clara St.

The traffic operations study completed in November 2018 provided an estimation of valet staffing needs to serve the valet demand of a proposed 330-room hotel with two designated valet parking spaces. The Nov. 2018 study identified the need to provide three to four valet staff to serve an estimated 84 vehicles during the PM peak period, which is the time period that the greatest number of vehicles may arrive to the hotel's valet area. The identified valet staff requirement presumed that a valet staff member would be available to serve the arrival of an estimated three vehicles every two minutes. In addition, it was recommended that vehicles should not be retrieved in advance of guests being present at the valet area and given the limited storage space for valet operations along Almaden Boulevard, the valet area should not be used for transportation network companies (TMCs) such as Uber, Lyft, etc. while waiting for customers.

The number of rooms of the proposed hotel has now been reduced from 330 to 272 rooms and the valet spaces to be provided increased from two to five spaces. The reduction in rooms will result in a reduction in vehicles that would be served at the valet area to 69 vehicles or approximately one to two vehicles each minute during the peak hours. Presuming three to four valet staff are present during the peak periods and arrival of no more than two vehicles per minute, the identified five valet spaces would provide adequate capacity for the storage and additional storage for two to three vehicles. However, it would still be recommended that vehicles should not be retrieved in advance of guests being present at the valet area given the limited storage space for valet operations along Almaden Boulevard and Santa Clara Street.

The valet area also should not be used for transportation network companies (TMCs) such as Uber, Lyft, etc. while waiting for customers.

Robert

Robert Del Rio, T.E.

Vice President & Principal Associate

Hexagon Transportation Consultants, Inc.

San Jose | Gilroy | Phoenix | Pleasanton

8070 Santa Teresa Boulevard, Suite 230 | Gilroy, California 95020

 408.846.7410 | Direct  408.846.7412 |  408.846.7418

www.hextrans.com

 Please consider the environment before printing this material

From: [Robert Del Rio](#)
To: [Shannon George](#)
Cc: [Le, Thai-Chau](#); [Villanueva, Arlyn](#)
Subject: RE: Almaden Corner Hotel
Date: Tuesday, August 6, 2019 7:07:17 AM

Please see responses below.

Robert

Robert Del Rio, T.E.

Vice President & Principal Associate

From: Shannon George <sgeorge@davidjpowers.com>
Sent: Monday, August 5, 2019 4:19 PM
To: Robert Del Rio <rdelrio@hextrans.com>
Cc: Le, Thai-Chau <Thai-Chau.Le@sanjoseca.gov>; Villanueva, Arlyn <Arlyn.Villanueva@sanjoseca.gov>
Subject: Almaden Corner Hotel

Robert,

I am sorry to bother you as I understand you are on vacation, but there are some outstanding items on this project. We would like your input as to how these issues affect the LTA.

1. The primary item is that the valet parking is now proposed on both street frontages. Since this was not the original proposal it is not reflected in the LTA. Does this change the distribution or queuing?

The additional valet space along Santa Clara would result in a change to the assignment of project traffic shown in the LTA. However, it would be a minimal amount of trips and would not result in new or worsen any issues in the completed LTA.

2. With the valet spaces on Santa Clara Street, the dedicated bike lane would be modified to a shared lane to allow the valet spaces. Would this require revisions to the LTA?

Not sure this is necessarily a project issue. There is currently metered parking on Santa Clara Street. I do not believe the planned bike lane would be a cycle track (bike lane separated from travel lane by parking). Thus, standard loading spaces will be required along Santa Clara Street for other development fronting Santa Clara Street.

3. The City is not certain if or when Almaden Boulevard may change to a two-way road. City staff are checking with DOT. We may want to remove discussion of this.

I believe the LTA was completed based on the current one way operations of Almaden. The conversion to a two-way street was only evaluated as an alternative.

4. If you have to modify the LTA, I would like to revise it based on the current room count of 272.

Let me know if you believe we will need to revise the LTA regardless of the minimal effect of the above issues.

Thanks,
Shannon

Shannon George | Principal Project Manager

[David J. Powers & Associates, Inc.](#)

[1871 The Alameda, Suite 200 | San José, CA 95126](#)

Main: 408.248.3500 | Direct: 408.454.3402

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